



Request for Statement of Interest (SOI)  
Walker Road over Burlington Creek

The Kane County Division of Transportation is in need of professional services from a qualified engineering firm to provide engineering services as detailed in the attached preliminary scope of work.

The County anticipates initiating this work in late 2013, with construction to take place in 2015.

A Statement of Interest shall be submitted **VIA KDOTQBS** no later than **4:00 P.M.** on **June 7<sup>th</sup>, 2013**, and should be addressed to **Michael Zakosek, P.E., Senior Project Manager.**

Statements of Interest received will be used by County engineering staff to develop a short-list of three (3) firms. The County will then submit a Request for Proposal (RFP) and schedule interviews with the short-listed firms.

For more information regarding the SOI, such as content and format of these items, please reference the QBS document found at <http://www.co.kane.il.us/dot/consultant/selectionProcess.pdf>. Also, the SOI shall be submitted in PDF format viewable with the latest version of Adobe reader.

If the respondent plans to utilize a sub-consultant for any portion of this work please note this on the submitted Statement of Interest.

Short-listed firms will be posted at [www.co.kane.il.us/dot](http://www.co.kane.il.us/dot). Click on the link labeled "Consultant Selection", then click on the link labeled "Consultant Selection Summary Table".

**A Statement of Interest (SOI) received after the above noted deadline will not be considered.**

**Walker Road over Hampshire Creek  
Scope of Work**

**PROJECT DESCRIPTION/PRELIMINARY SCOPE OF SERVICES**

This work consists of phase II design services needed to replace the Walker Road over Hampshire Creek bridge. The proposed work is detailed in the following project development report.

This work includes all design, permitting, coordination and plan preparation activities necessary to construct the project.

**PROJECT DEVELOPMENT REPORT  
Categorical Exclusion - Group II**

**WALKER ROAD (CH 46)  
OVER BURLINGTON CREEK**

**KANE COUNTY**

**SECTION NO. 08-00133-01-BR**

**PROJECT NO. BROS-0089(155)**

**JOB NO. P-91-272-09**



**April 25, 2013**

Prepared for:  
Kane County Division of Transportation

By:  
Wills Burke Kelsey Associates  
St. Charles, Illinois



Route: County Highway 46 County: Kane  
 Local Agency: Kane County Division of Transportation Project No. BROS-0089(155)  
 L.A. Section No.: 08-00133-01-BR Project Length: 1180 ft.  
 Street/Road Name: Walker Road (C.H. 46)  
 Termini: Over Burlington Creek (Touchdown to Touchdown)

For Township or Road District bridge projects: The County Engineer certifies that the project design speed exceeds the minimum design speed recommended for this classification of roadway as provided in the BLRS Manual in order to prevent a deficient NBIS rating for approach roadway alignment appraisal. All elements have been designed to the chosen design speed unless noted otherwise in Section 2(e) and/or the attached BLR 22120.

\_\_\_\_\_  
 County Engineer Date

Categorical Exclusion and Design Approval Recommended

  
 \_\_\_\_\_  
 Local Agency Date

4-25-13

\_\_\_\_\_  
 Regional Engineer Date

This project will not have any significant impacts on the human environment; therefore, the FHWA approves the project as a Categorical Exclusion on \_\_\_\_\_  
 Date

Design Approval

\_\_\_\_\_  
 Bureau of Local Roads & Streets Date

## 1. LOCATION AND EXISTING CONDITIONS

### a. **Location** (attach location map to supplement narrative description)

The Walker Road Bridge over Burlington Creek is located in unincorporated Kane County in Sections 19 and 20 of Hampshire Township (Tier 42 North, Range 6 East) west of the Village of Hampshire. The bridge is located approximately 800 feet south of Allen Road (C.H. 45). The project length is 1180 feet, from Sta. 3+20 to Sta. 15+00. See **Exhibit 1** for the Location Map.

### b. **Description of Existing Facility** - Give narrative description, including such items as width of through, parking and turn lanes, alignment, traffic control devices, utilities, jurisdiction, maintenance responsibility, drainage, terrain and current land use (including major public facilities and local landmarks). Attach existing typical sections showing roadway widths, bridge widths, ROW widths, curb and gutter and surface types.

#### **Terrain and Land Use**

The predominant land use is agriculture. The terrain is relatively flat. Burlington Creek flows from the southeast to the northwest near the middle of the project limits.

#### **Description of Roadway**

Walker Road (C.H. 46) is functionally classified by IDOT as a Minor Collector (Non-Urban). The roadway is not on the National Highway System nor is it classified as a Strategic Regional Arterial route. The Kane County Division of Transportation (KDOT) has jurisdiction and maintenance responsibility for the roadway and bridge.

The existing horizontal alignment of Walker Road within the project limits appears to consist of two tangents without curves. The deflection angle is approximately 0.375°.

The steepest grade within the project limits is approximately 1.5% located south of the bridge sloping toward the bridge. The existing profile grade is flat for approximately 200 feet at the bridge. North of the bridge, the grade is approximately 1.3% sloping toward the bridge. The vertical curves north of the bridge appears to be shorter than the 180-foot minimum for the 60 mph design speed but does not constrain sight distance.

The existing cross section of Walker Road within the project limits consists of two 12-foot hot-mix asphalt traffic lanes with 2 to 3-foot aggregate shoulders on both sides of the roadway. Portions of the shoulder are vegetated. The existing cross section over the bridge consists of two 12-foot hot-mix asphalt overlay traffic lanes (24 feet face-to-face). There are no turn lanes or sidewalks within the project limits. See **Exhibit 2** for the existing typical section.

A private driveway entrance is located at Sta. 3+48.50 RT. serving outbuildings of a farm. There exists a field entrance at Sta. 10+05 RT and another private entrance serving a residence is located just north of the project limits at Sta. 15+15 LT.

Parking is prohibited along Walker Road.

The existing right-of-way along Walker Road is 66 feet (**Exhibit 4** depicts the existing R.O.W.). The existing roadway is approximately centered about the 66-foot right-of-way. The section line (Sections 19 and 20) is incongruent with the roadway centerline and the center of the right-of-way.

#### **Drainage**

Burlington Creek is within the Kishwaukee River watershed of Kane County. The FEMA Flood Insurance Rate Map (FIRM) was reviewed (See **Exhibit 6**) and determined that the bridge and roadway are within the floodplain for several hundred feet north and south of the bridge. In the 100-year flood event, Burlington Creek overtops the roadway.

Stormwater runoff is conveyed primarily by roadside ditches toward Burlington Creek. For all quadrants, except the northeastern quadrant, the ditches end approximately 200 to 300 feet beyond the creek and runoff sheet flows through fields or wetlands. A drainage ditch in the northeast quadrant continues and outlets at the

creek. Culverts convey stormwater underneath a driveway entrance at Sta. 3+48.50 RT. and a field entrance at Sta. 10+05 RT.

**Utilities**

To identify utility facilities within the project limits, a design stage request was submitted to J.U.L.I.E. (Dig No. A1801496) which determined that the following companies have utilities within the project limits: AT&T, ComEd, and Nicor. Location information, including available atlases, was requested from these companies which were compared to field observations to confirm their general location. AT&T provided a response indicating a buried cable on the west side of the roadway. ComEd provided a letter indicating that they have 12kV power on overhead wires on the east side of the road within the project site. NICOR indicated that they do not have facilities within the project limits. See **Appendix F** for correspondence received from the utility companies. **Exhibit 4** depicts the known utility locations.

Nearby residences are on well and septic systems but will not be affected by the improvements.

**Traffic Control**

There are no intersections within the project limits. The intersection with Allen Road (C.H. 45) lies immediately north of the project limits. The intersection is 2-way stop controlled with Walker Road subject to the stop control.

**Roadway Lighting**

There is no roadway lighting within the limits of the project.

**Public Transportation Facilities**

The *RTA System Map, January 2011* was reviewed and there are no PACE bus routes along Walker Road within the project limits.

c. **Traffic Data**

Current ADT: 950 (2010) % trucks: 12

Will 80,000 trucks be legally permitted on this route?  Yes  No

Design Year: 2040 ADT: 6,000 DHV: 720 % trucks: 12

CMAP provided traffic projections (Refer to **Appendix H**).

- d. **Structures** - Identify location within the proposed improvement of all structures on attached location map. Attach a copy of the Structure Master Report for all structures within the project limits. Attach a copy of the Bridge Condition Report or the Bridge Deck Resurfacing approval letter for structures to be replaced, rehabilitated, or resurfaced.

The location of existing Structure No. 045-3036, is identified in **Exhibit 1**, Location Map. It is a two-span concrete slab bridge with closed abutments on spread footings with a length of 48'-11" back-to-back of abutments. The deck width is 28'-6" with a roadway width of 24 feet. The structure was built in 1946. In 2004, there were formed concrete repairs and additional approach guardrail was added. In 2005, stone riprap was added to protect the substructure. The bridge is skewed 35 degrees ahead left.

See *Master Structure Report* in **Appendix B**. The Bridge Condition Report was approved on November 15, 2010. The scope of work is bridge replacement. The *Preliminary Bridge Design and Hydraulic Report* was approved on October 24, 2012. See **Appendix B** for the *Bridge Condition Report* and *Preliminary Bridge Design and Hydraulic Report* approvals. See **Exhibit 2** for Existing Bridge Section.

- e. **Railroads** - Identify location of all railroad crossings on attached location map and complete the following:

There are no railroad crossings within or near the project limits.

- f. **Contiguous Sections** - Describe the existing typical sections at each end of the proposed improvement, including number of through lanes, turning lanes and parking lanes, lane widths and roadway width (f-f of curbs or e-e of shoulders).

The contiguous cross sections of Walker Road consist of two 12-foot hot-mix asphalt traffic lanes with 2 to 3-foot aggregate shoulders on both sides of the roadway. Portions of the shoulder are vegetated. Parking is prohibited along Walker Road.

## 2. Proposed Improvement

- a. Discuss the need and purpose of the project:

According to the Illinois Department of Transportation's "Master Structure Report" (see **Appendix B**) the bridge has a Sufficiency Rating of 25.7. The Sufficiency Rating is a numerical value used to evaluate data of the bridge by calculating four different factors: structural adequacy and safety; serviceability and functional obsolescence; essentiality for public use; and special reduction factors. Because the bridge has a Sufficiency Rating less than 50.0, the bridge is eligible for replacement under the Federal Highway Bridge Program (HBP). The FHWA concurred on September 11, 2012 with HBP Funding.

The purpose of this project is to replace this aging bridge (66 yrs.) which has several deficiencies with a bridge that has a safe load carrying capacity and can accommodate a minimum 1-foot free board.

- b. What design guidelines will be used for the proposed improvement? (Check One)

- Rural (BLRS Manual Chapter 32)  
 Urban (BLRS Manual Chapter 32)  
 3R Guidelines (BLRS Manual Chapter 33)  
 Bicycle Guidelines (BLRS Manual Chapter 42)

Functional Classification:       Arterial       Collector       Local Road       Other \_\_\_\_\_

Regulatory or Posted Speed Limit: 55 mph      Design Speed: 60 mph

The roadway is functionally classified by IDOT as a Minor Collector (Non-Urban).

- c. Describe type of work to be accomplished by the improvement. Discussion should include width of through, parking and turning lanes, traffic control devices, drainage items (including storm sewer outfalls), alignment changes, railroad work, utility adjustments, intersection improvements, side slopes and clear zones. Attach typical sections, plan and profile sheets and intersection design studies when applicable.

Refer to **Exhibit 3** for the Proposed Typical Sections and **Exhibit 4** for the Proposed Plan and Profile.

The existing bridge will be removed and replaced with a wider bridge. The proposed bridge will consist of wide-flange steel beams supported on integral abutments. The single span measures 73'-7" back to back of abutments and skewed 30° ahead left. The bridge will accommodate two 12-foot lanes and 4-foot shoulders for an out to out deck width of 32'-0". Type SM steel bridge railing will be provided and reinforced concrete approach slabs. The roadway approaches will be constructed with two 12-foot traffic lanes and 4-foot aggregate shoulders. Within the limits of the proposed guardrail, the shoulders will be 4-foot hot-mix asphalt from the edge of pavement to the face of the guardrail. The asphalt shoulder surface will extend 7 feet from the edge of pavement to 1-foot (min.) behind the back of the guardrail posts.

The proposed roadway pavement will be crowned at the centerline of the roadway and have cross-slopes of 2%. The shoulders will have a cross-slope of 4%. The proposed roadway alignment will follow the centerline of the proposed right-of-way.

The profile will be raised approximately 4 feet to provide adequate freeboard from the bridge low chord and the base flood elevation. Four proposed vertical curves, 2 crest and 2 sag are all designed for the 60 mph design speed. The K-values for all curves are adequate for the 60 mph design speed with the crest curve at VPI 8+25 with a K-Value of 160 and the sag curve at VPI 11+50 with a K-Value of 139 are near the minimum requirements.

The existing private and field entrances will be reconstructed. The field entrance will be constructed per the Kane County field entrance detail. The change in profile does not result in a reduction in intersection sight distances for these, or nearby entrances.

The proposed grading will provide better defined ditches on all four quadrants. Each will outlet into the creek near the bridge. Culverts will be replaced under the private and field entrances. The typical foreslopes will range from 1V:4H to 1V:3H and the back slopes will be 1V:3H, or flatter. Close to the bridge, the foreslopes will be 1V:2H. The width of the ditches will be 2 feet. In accordance with the Kane County Stormwater Ordinance, compensatory stormwater storage will be provided for this project, see **Section 6**, Floodplain Encroachment, for more information.

Within the limits of the proposed guardrail, typical side slopes will be 1V:3H, but will be as steep as 1V:2H at the abutments. Beyond the limits of the proposed guardrail, the typical side slopes will be 1V:4H. The limits of the guardrail were designed in accordance with guidelines presented in Chapter 35 in the BLR Manual.

There are no intersections to improve within the project limits, thus, there are no traffic control devices proposed.

No roadway lighting is proposed.

It appears that the proposed improvements will require the relocation of a minimum of two of the six ComEd power poles that are within the project limits. There are at least two locations where the poles and/or guy anchors would be in or near the proposed ditch bottom. Two poles will have increased cover and two poles will have a slight reduction in cover. Though the change in cover may not be considered a conflict, because at least two poles definitely will require relocation, it is preferred by the County that to have all six poles within the project limits relocated to the proposed right of way to maximize the clear zone and minimize future conflicts with maintenance operations. It appears an easement was granted on the east half of the right-of-way to Illinois Northern Utilities Company per Document Number 359915. Therefore, these relocation cost are expected to be the responsibility of the County.

It also appears that there will be conflicts with the underground AT&T cable. The cables lie within the grading limits of the proposed ditches which are being widened and deepened for drainage and compensatory storage from Sta. 3+50 to Sta. 5+00 (Lt.) and Sta. 10+50 to Sta. 15+00 (Lt.).

NICOR has no facilities within the project limits. Refer to **Appendix F** for utility coordination.

- d. Discuss items affecting improvement such as: hazardous mailbox supports, parking and truck restrictions, mail delivery from traffic lanes, justification (including warrants) for multi-way stop signs, traffic signals and other traffic control and railroad protective devices, stage construction, nearby airports, encroachments upon ROW and levels of illumination (if lighting will be provided):

No on-street parking is allowed on Walker Road, therefore, parking will not be provided.

There are no roadside mailboxes located within the project limits.

The proposed project will be constructed utilizing a detour.

Lighting is currently not provided and is not proposed.



There are no intersections within the limits of the project, therefore no traffic control is proposed.

There are no encroachments upon the existing or proposed right-of-way.

There are no nearby airports.

- e. Identify each aspect to be constructed at less than the design guidelines and provide a clear description of required variances and appropriate justification. (BLRS Manual Section 27-7)

The following design variances were presented and approved during the September 11, 2012 FHWA Coordination Meeting. **See Appendix D** for the Meeting Minutes.

**Shoulder Width** - Shoulder widths of 4 feet are proposed. The design criteria call for 8-foot shoulders. The narrower shoulder widths are proposed for several reasons. The existing shoulders are less than 4 feet wide within the project limits and for the adjacent segments of Walker Road. Eight-foot shoulders would require additional floodplain fill in an area that has limited availability to provide compensatory storage. Lastly, narrower shoulders reduce the amount of wetland impacts and minimize the amount of right-of-way take.

**Side Slope** - Side slopes steeper than the 1V:4H (BLR Figure 32-2A) are proposed. Side slopes will be typically 1V:3H behind guardrail and 1V:2H near the bridge abutments. The use of these side slopes is typical for bridge replacement projects and is appropriate for this location, because it reduces floodplain fill, wetland impacts and right-of-way take.

IDOT design policy states that “the clear zone should not be achieved at all cost”. Therefore, the clear zone to be provided is not presented as a design variance. Objects such as utility poles will remain within the 30-foot clear zone (BLR Fig. 35-2A) because it would require acquisition of additional right-of-way to achieve the full clear zone. Any ComEd poles that require relocation will be moved to the edge of the proposed right-of-way to maximize the clear zone to approximately 27 feet. It is recommended that all ComEd poles within the project limits be relocated. It should be noted that the lengthening of the guardrail will result in an overall improvement in shielding roadside hazards.

- f. Current estimated cost of proposed improvement? **\$1,390,000**

See **Exhibit 7** for the Opinion of Probable Construction Cost. The cost does not include design engineering, construction engineering, right-of-way or temporary easements. It includes \$70,000 for the anticipated relocation cost of ComEd facilities.

- g. Analyze the need for accommodating pedestrians, bicyclists and the handicapped. When applicable, describe the facilities to be provided including route continuity for the handicapped and marked crosswalk locations. (BLRS Manual Chapter 41)

There are no existing sidewalks or paths within the project limits. Although there are future trails indicated for the Walker Road corridor in the Kane County Bicycle Planning Map; because there are no immediate or future plans to improve the corridor, accommodations for pedestrians, bicyclists and the handicapped are not included in this project.

- h. Discuss any proposed improvements being considered in adjacent segments:

There are no other improvements proposed.

3. **Crash Analysis (BLRS Manual Section 22-2.11(b)(9))**

- a. Summarize crash data for the past three years, including a spot map or a location map showing crash locations when possible. Detail the types of crashes and include collision diagrams, if possible, especially at cluster sites. Give the source of this data.

A search of crash reports on Walker Road between Ill. Rte. 72 and Allen Road from 2007 through 2011 in the KDOT crash database yielded no results. The nearest crashes were at the intersections of IL. Rte.72 and Allen Road.

- b. Analyze available crash data including results of field check. Discussion should include high crash locations, critical wet weather sites, and other crash patterns. If the data is inconclusive make a statement to that effect.

The crash data was inconclusive since the data indicated no crashes occurring during the reporting period. A field visit did not identify any unusual existing conditions that may increase the risk for crashes.

- c. Describe proposed countermeasures.

Since the data indicated no crashes occurring during the reporting period, and a field visit did not identify any unusual existing conditions that may increase the risk for crashes, no countermeasures are proposed.

4. **Right-of-Way**

- a. Describe the right-of-way taking, including the total area required for each of the following categories: ROW, permanent easements, temporary easements and temporary land use permits. Include: width of taking, number of property owners, character of land; i.e., farm, residential, commercial or publicly owned properties, anticipated effects on properties to remain and location of any improvements with respect to required right-of-way. Discuss any effects on setbacks required by zoning.

Near the southerly end of the project limits the width of the right-of-way take will be a minimum of 7 feet on both sides of the roadway resulting in a total minimum 80-foot right-of-way width. Approaching the bridge, the proposed right-of-way will widen to 110 feet, centered about the roadway centerline. North of the bridge, the proposed right-of-way will narrow to 90 feet, centered about the roadway centerline. The total area of taking will be approximately 0.92 acre from 2 parcels. This right-of-way is necessary to accommodate oversized ditches to provide compensatory stormwater storage within the proposed right-of-way.

Temporary construction easements are required from two parcels to accommodate grading, channel excavation, utility relocations and construction equipment near the bridge. The widths of the temporary easements are 5 feet. The total area of temporary easements will be approximately 0.18 acre. The land use of the affected parcels is agricultural.

See **Exhibit 8**, Summary of Right-of-Way and Easement Acquisition for P.I.N.s, width of taking, and other details.

- b. Are any persons, businesses or farms to be displaced?

Yes       No

5. **Prime Farmland (BLRS Manual Section 20-10)**

- a. If the project requires more than 3 acres/mile (0.75 hectares/kilometers), 10 acres (4 hectares) for a non-linear improvement, or the project ROW is not contiguous to the existing ROW, contact the Illinois Department of Agriculture and attach results of the coordination.

This project is not subject to the coordination with the Illinois Department of Agriculture because it is a bridge project, which is considered a non-linear project that requires less than 10 acres of right-of-way.

- b. When a project requires consultation with the Natural Resource Conservation Service (Letter #85-19), fill out Form AD-1006 and submit it to the local office of NRCS. Attach the completed form; otherwise explain why the project is exempt from consultation.

This project is not subject to the coordination with the Natural Resource Conservation Service because it is a bridge project, which is considered a non-linear project that requires less than 10 acres of right-of-way.

**6. Floodplain Encroachment (BLRS Manual Section 20-7)**

Does the proposed work cross or encroach upon a 100-year floodplain, including a regulatory floodway?

Yes       No

If yes, summarize the location hydraulics study, regulatory floodway restrictions, the effect of any encroachment (including a comparison between existing and proposed conditions) and the effect of over-the-road flow on the proposed transportation facility. Attach any available floodplain maps.

The project encroaches on the 100-year Zone A floodplain of Burlington Creek; see **Exhibit 6** for the FEMA Flood Insurance Rate Map. See **Exhibit 9** for IDNR-OWR Permit No. NE2013002.

Per Article 410 of the Kane County Stormwater Ordinance, floodplain fill and compensatory storage were calculated for this project. The floodplain fill and compensatory storage volumes were calculated based on the hydraulic analysis prepared by Wills Burke Kelsey Associates. Kane County requires all floodplain fill to be compensated at a 1.5:1 ratio with a minimum 1:1 ratio within the Normal to 10-year flood elevation, and a minimum 1:1 ratio within the 10-year to 100-year flood elevation. The additional 0.5:1 can be provided in either the Normal to 10-year or 10-year to 100-year floodplains.

The proposed fill will be 427 CY in the Normal to 10-year flood plain and 346 CY 10-year to 100-year flood plain. The compensatory storage to be provided will be 840 CY in the Normal to 10-year flood plain and 372 CY 10-year to 100-year flood plain. Most of the compensatory storage is to be provided in roadside ditches with the remaining to be the result of a larger bridge opening.

There will be no fill in the floodway; therefore no IDNR-OWR floodway compensatory storage requirements are required.

The Preliminary Bridge Design and Hydraulic Report was approved on October 24, 2010. See **Appendix B**.

**7. Phase I & II NPDES Storm Water Permit Requirements (BLRS Manual Section 7-4.01)**

Will the project involve soil disturbance of 1 acre (0.4 hectares) or more?

Yes       No

This project must comply with the Phase II NPDES Storm Water Permit requirements. A Kane County Stormwater Permit will be required for this project, to be completed during Phase II.

**8. "404" Permit (BLRS Manual Section 7-4.02)**

If this project involves water regulated by Section 404, is the project covered by a nationwide permit?

Yes       No

A pre-application meeting with the USACE was held on October 18, 2010 (see **Appendix C** for the meeting minutes). Based on discussions the project is anticipated to be processed under the Regional Permit Program (RPP): Permit 3 and Permit 7. The permitting process will be completed during Phase II.

9. **Special Waste (BLRS Manual Section 20-12)**

- a. Following the special waste assessment screening criteria shown on Figure 20-12A of the BLRS Manual, is Preliminary Environmental Site Assessment (PESA) required?

Yes  No

The land has always been agricultural, residential or undeveloped, and there are no LUST or CERCLIS sites, and that there are no visible signs of contamination, there is low risk for special waste. Therefore, a PESA is not required based on the screening criteria.

- b. If PESA is required, is special waste located on property to be acquired in the name of the state or are contract plans being prepared by the state?

Yes  No  N/A

- c. If PESA is required, did the PESA results determine that the project is a "moderate" or "high" risk for special waste?

Yes  No  N/A

10. **Environmental Survey (BLRS Manual Section 20-2)**

Whenever a project involves land acquisition (including easements), any in-stream work (including drainage structure run-around), or is located within or adjacent to historic properties listed in (or eligible for) the National Register of Historic Places, wetlands or known locations of threatened or endangered species, the Environmental Survey Request Form should be submitted early in the project development phase.

See the "Project Overview" Form in **Appendix A** for the environmental signoffs:

<b>Resource</b>	<b>Cleared For Letting</b>
Biological	November 4, 2010
Cultural	July 6, 2011
Wetlands	January 10, 2013

- a. Wild and Scenic Rivers - If this project crosses or affects a river on the National Wild and Scenic Rivers System or a river listed in the Nationwide Inventory of Rivers with potential for inclusion on the system, include coordination between the National Park Service and the Bureau of Design and Environment (BDE).

Involvement  No Involvement

- b. Wetlands - If the proposed work involves the use of regulatory wetlands, prepare a "wetlands study" describing the wetlands taking, avoidance minimization and any mitigation measures. Include results of coordination.

Involvement  No Involvement

A Wetland Assessment Report and Wetland Impact Evaluation (WIE) form were submitted to IDOT on September 24, 2012. The wetlands have been cleared for design approval and letting on (*Pending*). See **Appendix A-3** for the approval letter).

In summary, the wetland impacts will be mitigated by the purchase of credits from an in-basin wetland bank site using a mitigation ratio of 1.5:1. Thus, the proposed wetland impact of 0.218 acre will require a purchase of 0.327 acre of credit will be required prior to construction.

- c. Archaeological and Historical Preservation - Include copy of cultural resources clearance by BDE, SHPO or ACHP.  
 Involvement  No Involvement
  
- d. Threatened or Endangered Species - Include copy of biological resources memorandum or signoff by BDE.  
 Involvement  No Involvement
  
- e. Stream Modification and Wildlife Impacts - Include copies of any correspondence between BDE and IDOC or U.S. Fish and Wildlife Service. Attach copies of any additional coordination between local agency and IDOC or U.S. Fish and Wildlife Service whenever required as a result of biological review by BDE. Address any proposed mitigation measures.  
 Involvement  No Involvement

11. **Section 4(f) Lands (BLRS Manual Section 20-3)**

Does this project require any right-of-way, including temporary construction easements, from a publicly owned park, recreational area, wildlife and waterfowl, or any historic site in or eligible for the National Register of Historic Places?

Yes  No

The project does not require right-of-way or easement acquisition from any such site.

12. **Air Quality (BLRS Manual Section 20-11) Check One:**

- a.  This project is in an attainment area.
  
- Projects within a portion of a nonattainment area for which the Chicago Metropolitan Agency for Planning (CMAP) is the MPO.

This project is included in the 2030 RTP (Regional Transportation Plan) and in the Transportation Improvement Program (TIP), endorsed by the CMAP, the region's Metropolitan Planning Organization. The 2030 RTP (Regional Transportation Plan) was found to conform by the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) on 10/20/2003.

The TIP was found to conform by the FHWA on 10/16/2006 and by the FTA on 10/16/2006.

The TIP Number for this project is 09-08-0038.

b. **Mobile Source Air Toxics (See BDE PM 52-06)**

This project will not result in any meaningful changes in traffic volumes, vehicle mix, location of the exiting facility, or any other factor that would cause an increase in emissions relative to the no-build alternative. As such, FHWA has determined that this project will generate minimal air quality impacts for Clean Air Act criteria pollutants and has not been linked with any special Mobile Source Air Toxic concerns. Consequently, this effort is exempt from analysis for MSATs.

Moreover, EPA regulations for vehicle engines and fuels will cause overall MSATs to decline significantly over the next 20 years. Even after accounting for a 64 percent increase in VMT, FHWA predicts MSATs will decline in the range of 57 to 87 percent, from 2000 to 2020, based on regulations now in effect, even with a projected 64 percent increase in VMT. This will both reduce the background level of MSATs as well as the possibility of even minor MSAT emissions from this project.

c. **Construction-related Particulate Matter**

Demolition and construction activities can result in short-term increases in fugitive dust and equipment-related particulate emissions in and around the project area. (Equipment-related particulate emissions are usually insignificant when equipment is well maintained.) The potential air quality impacts will be short-term, occurring only when demolition and construction work is in progress and local conditions are appropriate.

The potential for fugitive dust emissions typically is associated with building demolition, ground clearing, site preparation, grading, stockpiling of materials, on-site movement of equipment, and transportation of materials. The potential is greatest during dry periods, periods of intense construction activity, and during high wind conditions.

The Department's Standard Specifications for Road and Bridge Construction include provisions on dust control. Under these provisions, dust and airborne dirt generated by construction activities will be controlled through dust control procedures or a specific dust control plan, when warranted. The contractor and the Department will meet to review the nature and extent of dust-generating activities and will cooperatively develop specific types of control techniques appropriate to the specific situation. Techniques that may warrant consideration include measures such as minimizing track-out of soil onto nearby publicly-traveled roads, reducing speed on unpaved roads, covering haul vehicles, and applying chemical dust suppressants or water to exposed surfaces, particularly those on which construction vehicles travel. With the application of appropriate measures to limit dust emissions during construction, this project will not cause any significant, short-term particulate matter air quality impacts.

d. **Project-level Hot Spot Analysis. Check One:**

- This project is in an attainment area and does not require a hot spot analysis.
- This project does not meet the definition of a project of air quality concern as defined in 40 CFR 93.123(b)(1) due to low ADT and low percentage of diesel trucks. It has been determined that the project will not cause or contribute to any new localized PM<sub>2.5</sub> or PM<sub>10</sub> violations or increase the frequency or severity of any PM<sub>2.5</sub> or PM<sub>10</sub> violations. The USEPA has determined that such projects meet the Clean Air Act's requirement without any further Hot-Spot analysis.
- This project is in a non-attainment or maintenance area and is a project of air quality concern. Therefore, a qualitative hot spot analysis is required (see Attachment).

e. **COSIM**

Are through lanes or auxiliary turn lanes being added with this project?

- Yes       No

No COSIM analysis is required.

13. **Noise (BLRS Manual Section 20-6)**

If this project involves a new alignment, additional lanes, or involves a significant alignment change, attach a traffic noise analysis.

There are no proposed changes in the horizontal alignment and there are no proposed capacity improvements. Therefore, no traffic noise analysis has been performed.

14. **Maintenance of Traffic (BLRS Manual Section 22-2.11(b)(9))**

Discuss how vehicle traffic and pedestrians will be accommodated during construction, including the effect of any road closure and sidewalk removal. If the road will be closed, include information concerning location of alternate routes and their ability to handle the additional traffic (street width, number of traffic lanes, structural adequacy, etc.)

To reduce construction time and reduce the overall travel impacts to the public, it is proposed that Walker Road, in the vicinity of the project, be closed during construction and traffic detoured around the site (see **Exhibit 5- Detour Route Map**). The duration of the closure of Walker Road is estimated to last three months. To avoid impacts to school bus routes, most of the closure would be scheduled during the summer months while school is not in session. The detour route will be marked on State, County, and Local highways as described below.

- Northbound traffic will be directed east on Illinois Route 72, then north on State Street (Hampshire), and then west on Allen Road (C.H. 45). The distance of the proposed detour is approximately 5.3 miles.
- Southbound traffic will be routed in the opposite direction over the same route.

Because the detour route includes a State Route, a *Detour Report* was prepared and submitted to IDOT. The *Detour Report* was reviewed by IDOT. In a memo dated 11/12/10, IDOT Bureau of Traffic indicated that there was no objection to the proposed route. Minor comments regarding the signage will be addressed in Phase II, when the final Detour Plan is to be prepared. The memo also indicated that the Bureau of Programming should be notified to determine whether there are proposed projects on Ill. Rte 72. The report was forwarded and a memo signed by the Bureau of Programming on 3/9/11 indicates there are plans to signalize IL. Rte. 72 at State Street. The anticipated letting is in June 2013, contingent on approval of preliminary engineering and environmental studies by December 2011. There is no indication that there should be a restriction on constructing these projects simultaneously, however, as both projects advance, there should be coordination so that if the projects occur simultaneously, certain activities such as locating and relocating detour signs, should be coordinated. See **Appendix E** for the IDOT and the Village of Hampshire's approval.

15. **Public Involvement (BLRS Manual Chapter 21)**

- a. Summarize informational meetings, council or board meetings, media coverage and personal contact with public.

As recommended during the September 11, 2012 FHWA Coordination Meeting, letters were sent to the parcel owners outlining the project, the proposed amount of right-of-way and easements, and an overview of the appraisal and negotiation process. Letters were sent to the following:

William L. Dumoulin, 16N393 Walker Road, Hampshire IL (PIN 01-19-200-003)  
Melanie J. Howe, 16N731 Walker Road, Hampshire IL (PIN 01-20-100-001)

A copy of the letters and responses are included in **Appendix G**.

- b. Has any opposition been expressed toward the improvement?  
 Yes       No

- c. How are public hearing requirements being fulfilled?

A Notice of Public Hearing Offer was advertised in the Elgin Courier News on 2/28/2013 & 2/15/2013. No requests were received; therefore, no public hearing was held.

A Certificate of Publication is included in **Appendix G**.

16. **Coordination: LA-IDOT-FHWA (BLRS Manual Section 22-1.02)**

The initial coordination meeting was held on September 11, 2012. Meeting minutes of coordination meetings are included in **Appendix D**.

17. **Other Coordination**

Attach results.

None

18. **Summary of Commitments**

1. The Joint Permit (USACE) will be completed during Phase II. This will require coordination with the Kane-DuPage Soil and Water Conservation District.
2. A Kane County Stormwater Permit will be completed during Phase II.
3. Wetlands impacts will be mitigated off site and credits purchased in Phase II.
4. Trees will be replaced in Phase II in accordance with the provisions of IDOT's tree replacement Policy D&E-18.



## SUMMARY OF ATTACHMENTS

### Exhibits

Exhibit 1	Location Map
Exhibit 2	Existing Typical Sections
Exhibit 3	Proposed Typical Sections
Exhibit 4	Proposed Plan and Profile
Exhibit 5	Proposed Detour Map
Exhibit 6	Flood Insurance Rate Map (FIRM)
Exhibit 7	Opinion of Probable Construction Cost
Exhibit 8	Summary of Right-of-Way and Easement Acquisition
Exhibit 9	IDNR-OWR Floodway Permit

### Appendix A- Environmental Coordination

Project Overview Form	A-1
Biological Resources Coordination	A-2
Cultural Resources Coordination	A-4
Wetlands Resources Coordination	A-5

### Appendix B- Structural and Hydraulic Approvals

Master Structure Report	B-1
Bridge Condition Report Approval Letter	B-3
Preliminary Bridge Design & Hydraulic Approval	B-4

### Appendix C- Permit Coordination

USACOE Pre-Application Meeting Minutes	C-1
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### Appendix D- FHWA Coordination

FHWA Meeting Minutes	D-1
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### Appendix E- Detour Coordination

IDOT Approval	E-1
Village of Hampshire Approval	E-2

### Appendix F- Utility Coordination

JULIE Design Stage Ticket	F-1
AT&T Response	F-2
ComEd Response	F-3
NICOR Response	F-5

### Appendix G- Public Involvement

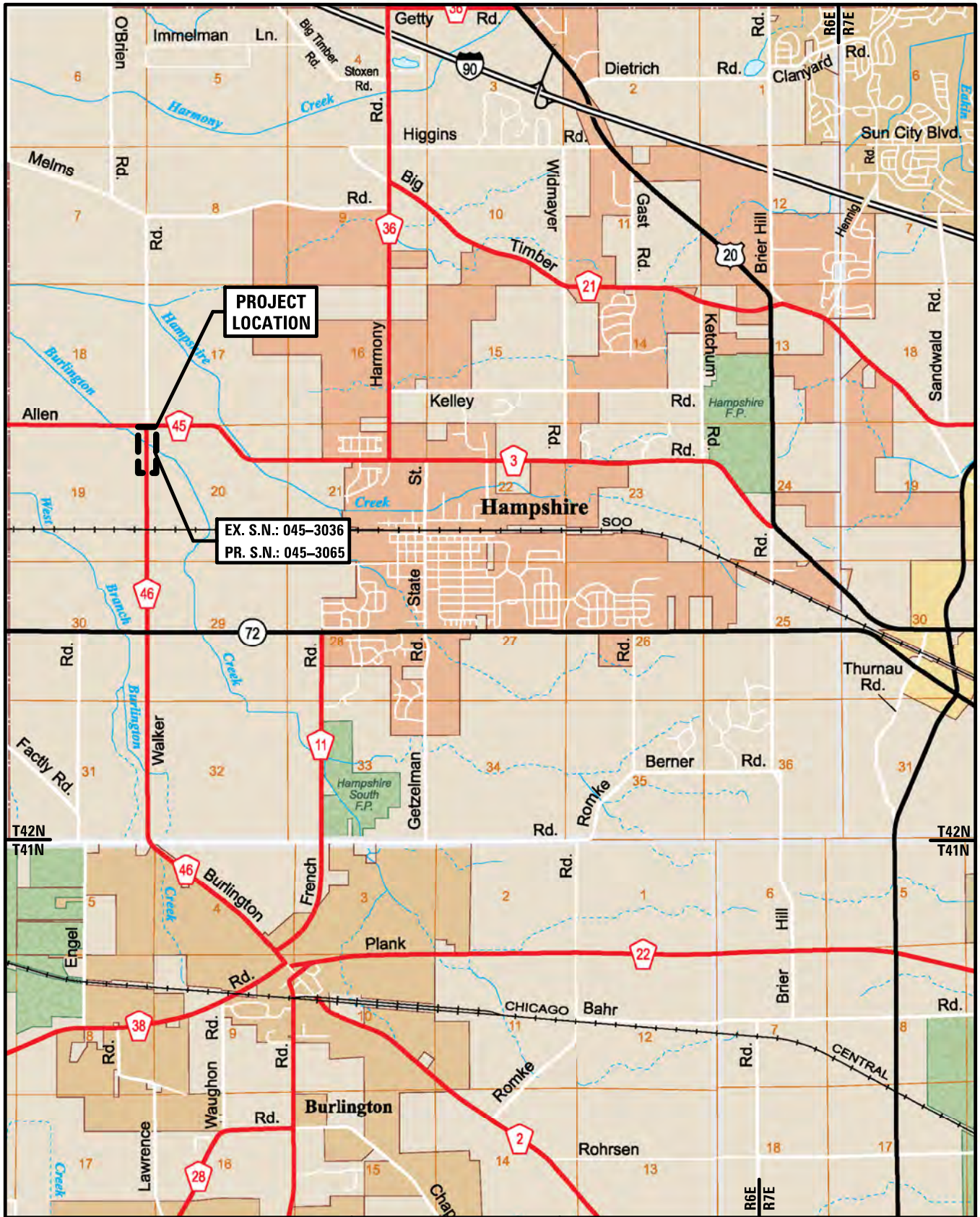
Certificate of Publication	G-1
Property Owner Letters	G-3

### Appendix H- Other Agency Coordination

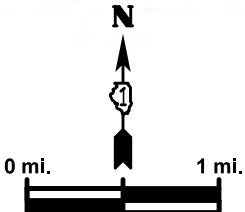
CMAP Traffic Projection	H-1
USGS	H-2
Kane County Division of Facilities, Subdivision, and Environmental Resources	H-3

## **EXHIBITS**

Exhibit 1	Location Map
Exhibit 2	Existing Typical Sections
Exhibit 3	Proposed Typical Sections
Exhibit 4	Proposed Plan and Profile
Exhibit 5	Proposed Detour Map
Exhibit 6	Flood Insurance Rate Map (FIRM)
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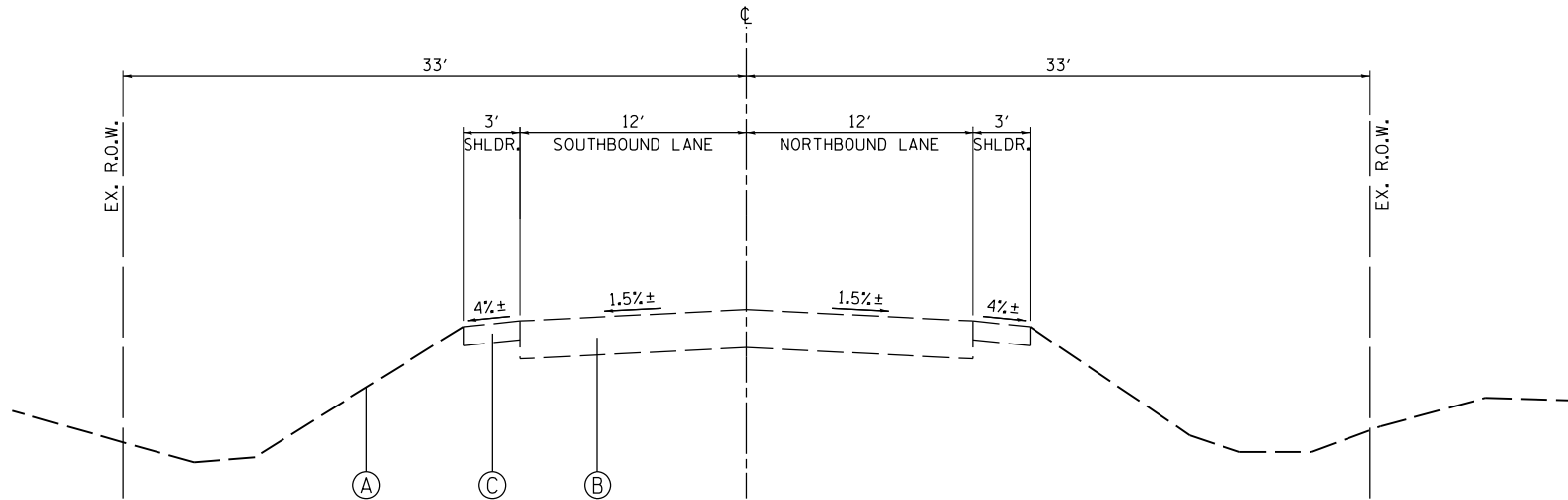



**WILLS BURKE KELSEY ASSOCIATES LTD.**  
 116 West Main Street, Suite 201  
 St. Charles, Illinois 60174  
 (630) 443-7755



**LOCATION MAP**  
**WALKER RD. OVER BURLINGTON CREEK**  
**KANE COUNTY**  
**SEC. 08-00133-01-BR**

**EXHIBIT 1**



**EXISTING TYPICAL SECTION**  
**STA. 3+20 TO STA. 6+65 LT./STA. 5+25 RT.**  
**STA. 11+20 LT./STA. 9+75 RT. TO STA. 15+00**

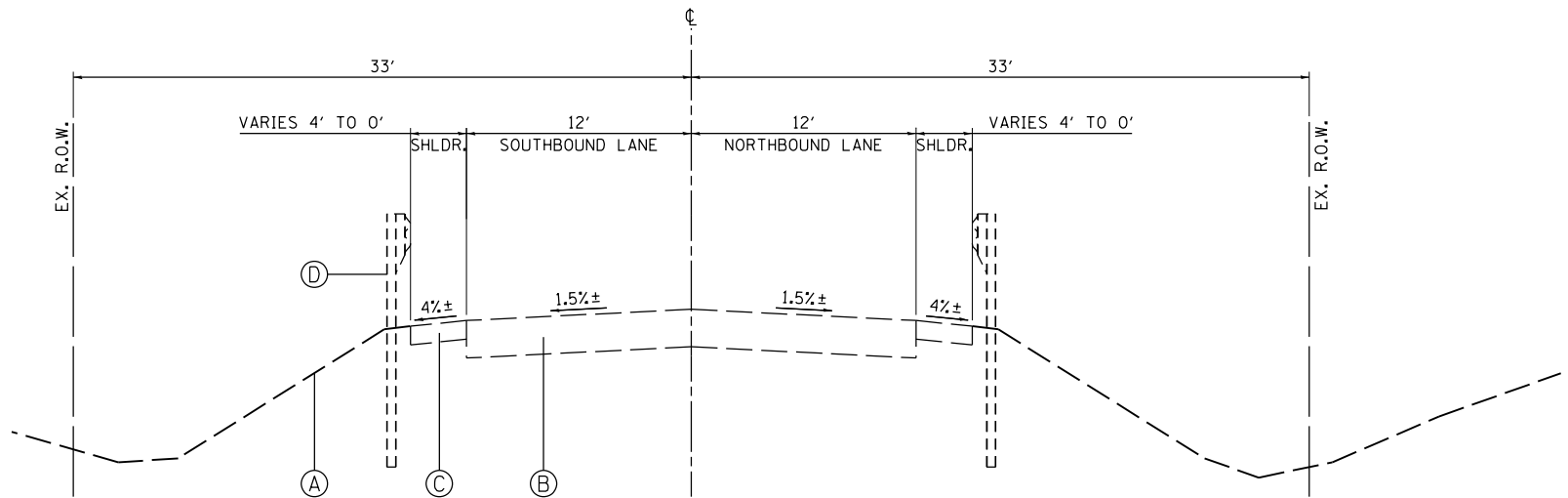
**LEGEND**

- (A) EXISTING GROUND
- (B) PAVEMENT, HOT-MIX ASPHALT
- (C) SHOULDER, AGGREGATE

**EXISTING TYPICAL SECTION**

**WALKER RD. OVER BURLINGTON CREEK**  
**KANE COUNTY**  
**SEC. 08-00133-01-BR**

**EXHIBIT 2-1**



**EXISTING TYPICAL SECTION**  
**STA. 6 + 65 LT./STA. 5 + 25 RT.**  
**TO STA. 8 + 03 LT./STA. 7 + 87 RT.**

**STA. STA. 8 + 53 LT./STA. 8 + 38 RT.**  
**TO STA. 11 + 20 LT./STA. 9 + 75 RT.**

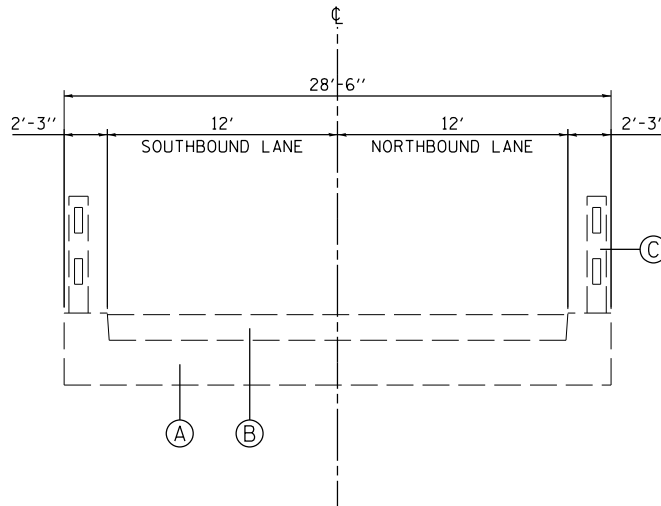
**LEGEND**

- (A) EXISTING GROUND
- (B) PAVEMENT, HOT-MIX ASPHALT
- (C) SHOULDER, AGGREGATE
- (D) GUARDRAIL

**EXISTING TYPICAL SECTION**

**WALKER RD. OVER BURLINGTON CREEK**  
**KANE COUNTY**  
**SEC. 08-00133-01-BR**

**EXHIBIT 2-2**



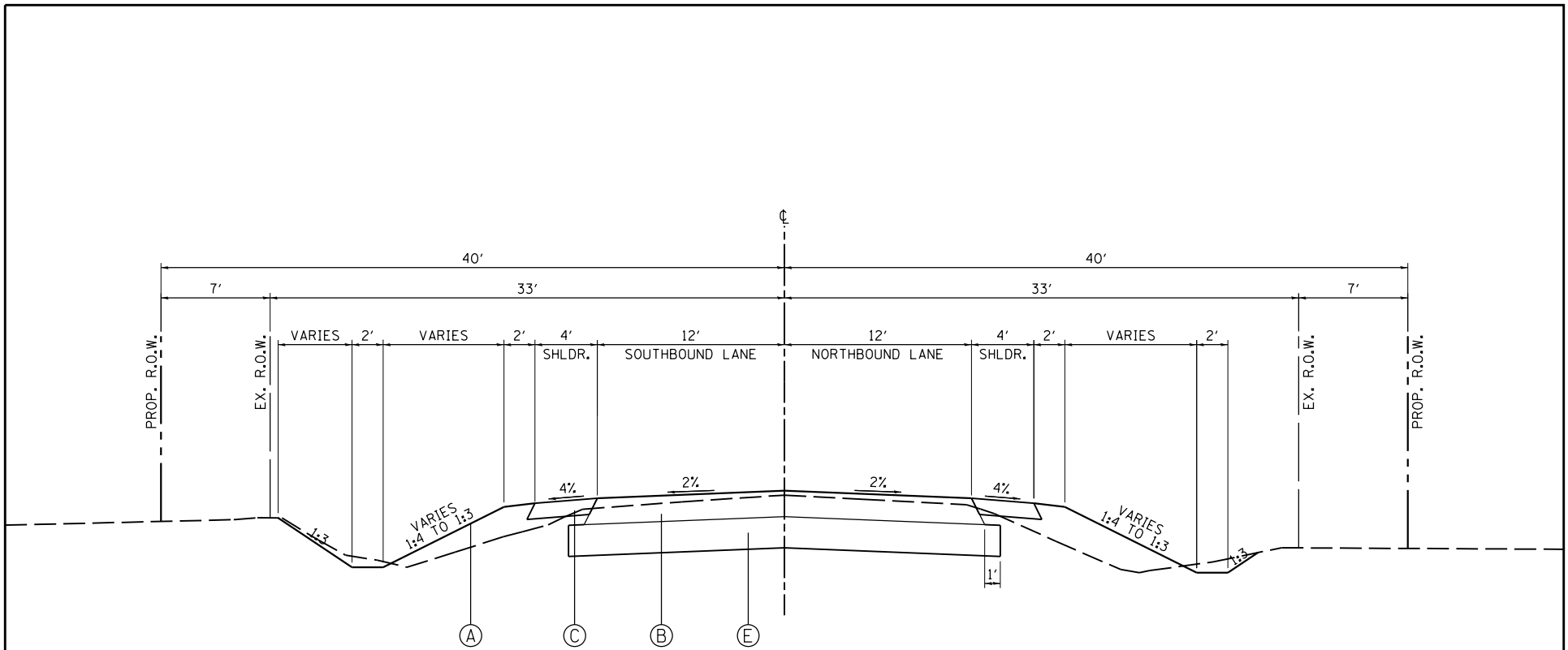
**EXISTING TYPICAL SECTION  
 STA. 8 + 03 LT./STA. 7 + 87 RT.  
 TO STA. STA. 8 + 53 LT./STA. 8 + 38 RT.  
 (BRIDGE DECK)**

**LEGEND**

- (A) R.C SLAB
- (B) BITUMINOUS OVERLAY
- (C) CONCRETE BRIDGE RAILING

**EXISTING TYPICAL SECTION**

**WALKER RD. OVER BURLINGTON CREEK  
 KANE COUNTY  
 SEC. 08-00133-01-BR**



**PROPOSED TYPICAL SECTION  
STA. 3+20 TO STA. 4+13 (+/-)**

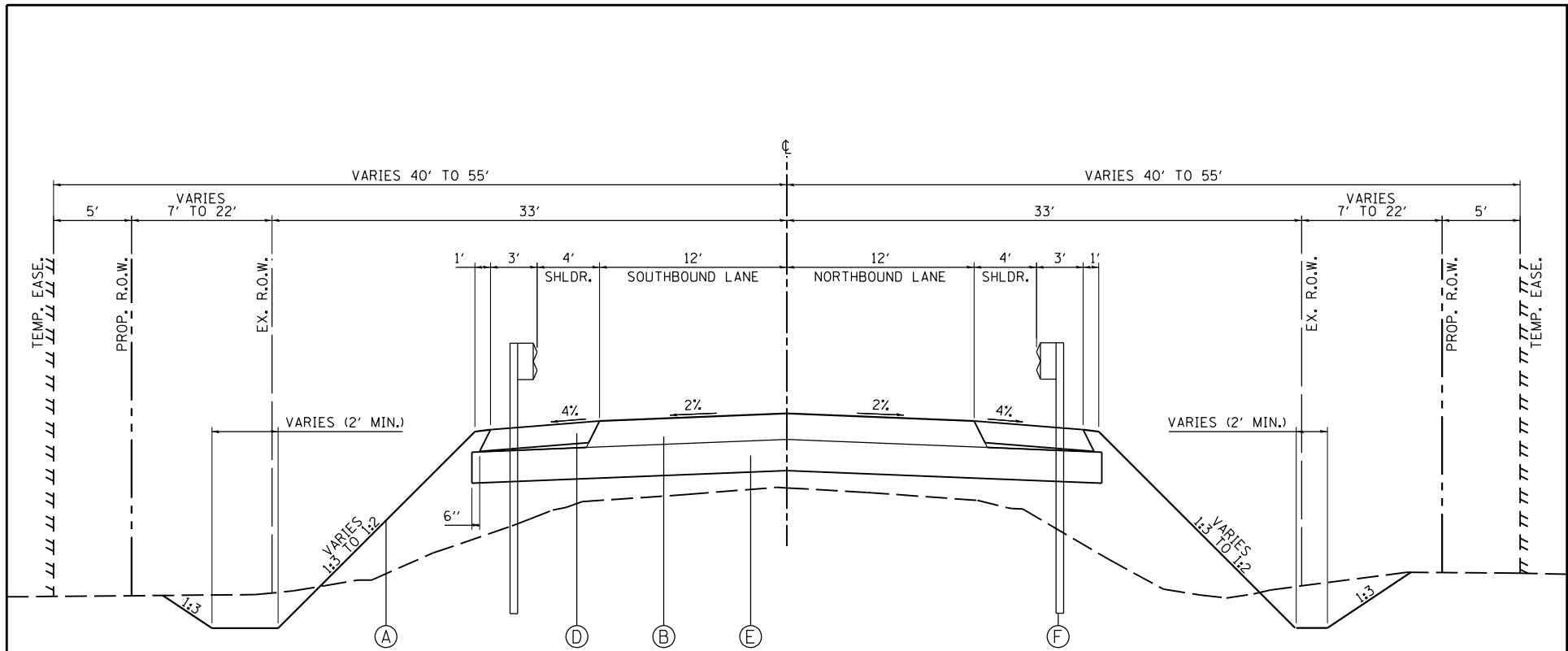
**LEGEND**

- |                               |                        |
|-------------------------------|------------------------|
| (A) PROP. GROUND              | (E) AGGREGATE SUBGRADE |
| (B) PAVEMENT, HOT-MIX ASPHALT | (F) GUARDRAIL          |
| (C) SHOULDER, AGGREGATE       |                        |
| (D) SHOULDER, HOT-MIX ASPHALT |                        |

**PROPOSED TYPICAL SECTIONS**

**WALKER RD. OVER BURLINGTON CREEK  
KANE COUNTY  
SEC. 08-00133-01-BR**

**EXHIBIT 3-1**



**PROPOSED TYPICAL SECTION**  
**STA. 4 + 13 LT. (+/-) TO STA. 11 + 83 LT. (+/-) (OMIT BRIDGE)**  
**STA. 4 + 13 RT. (+/-) TO STA. 10 + 39 RT. (+/-) (OMIT BRIDGE)**

NOTE: DITCHES ARE OVERSIZED TO PROVIDE COMPENSATORY STORMWATER STORAGE. ALL COMPENSATORY STORAGE IS TO BE PROVIDED WITHIN THE PROPOSED RIGHT-OF-WAY.



**LEGEND**

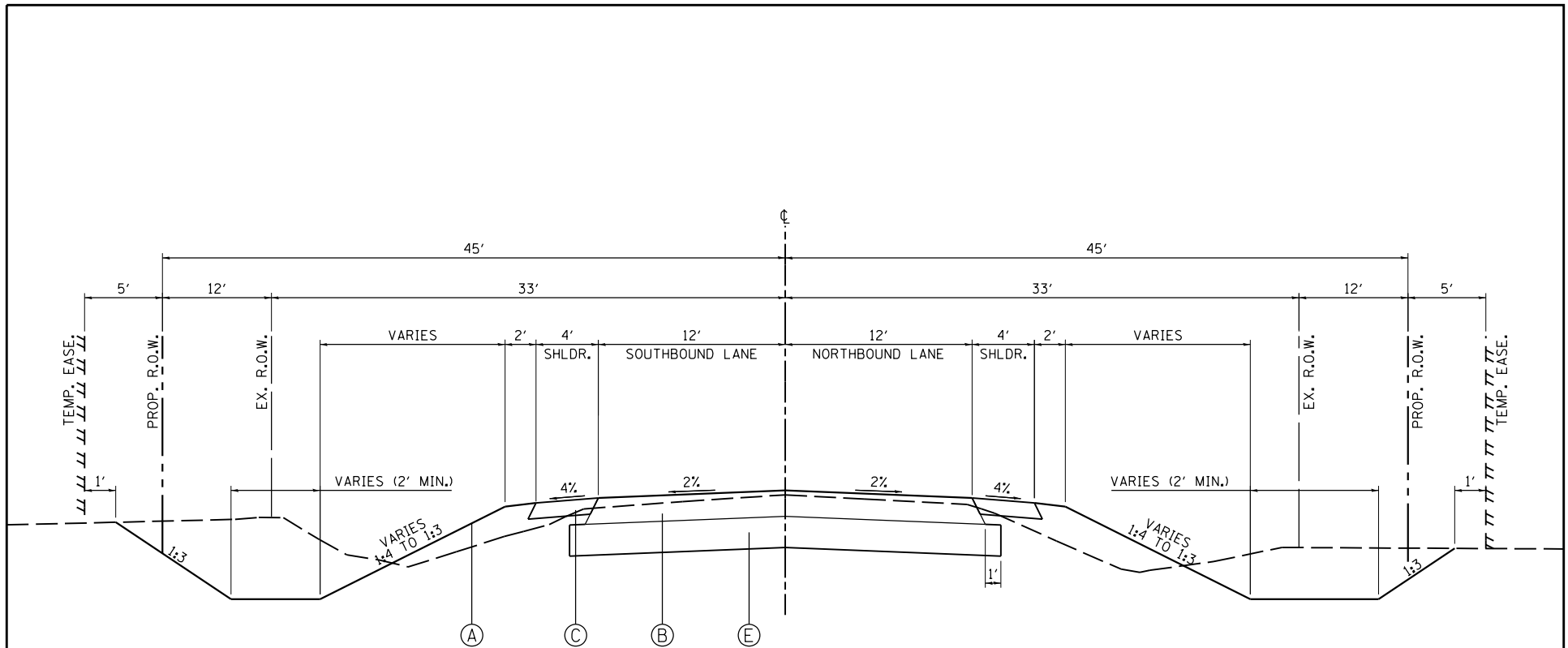
- |                               |                        |
|-------------------------------|------------------------|
| (A) PROP. GROUND              | (E) AGGREGATE SUBGRADE |
| (B) PAVEMENT, HOT-MIX ASPHALT | (F) GUARDRAIL          |
| (C) SHOULDER, AGGREGATE       |                        |
| (D) SHOULDER, HOT-MIX ASPHALT |                        |

**PROPOSED TYPICAL SECTIONS**

**WALKER RD. OVER BURLINGTON CREEK**  
**KANE COUNTY**  
**SEC. 08-00133-01-BR**

**EXHIBIT 3-2**





**PROPOSED TYPICAL SECTION**  
**STA. 11+83 LT. (+/-) TO STA. 15+00 LT. (+/-)**  
**STA. 10+39 RT. (+/-) TO STA. 15+00 RT. (+/-)**

NOTE: DITCHES ARE OVERSIZED TO PROVIDE COMPENSATORY STORMWATER STORAGE. THE 100-YEAR FLOOD ELEVATION WILL NOT SPILL PAST THE PROPOSED RIGHT-OF-WAY.



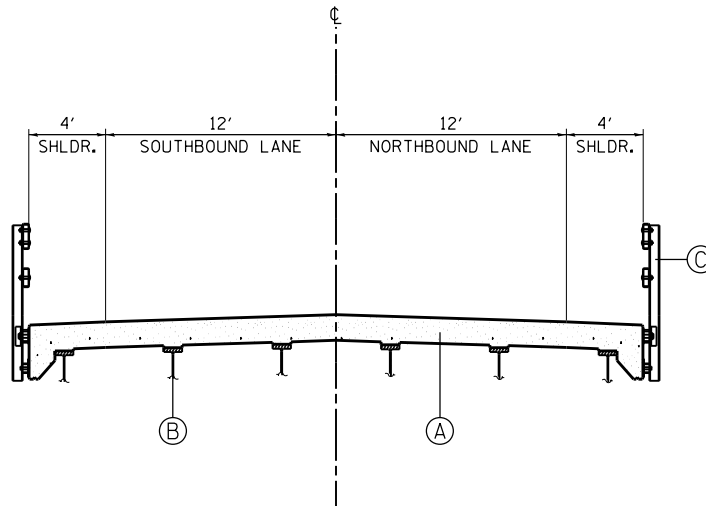
**LEGEND**

- |                               |                        |
|-------------------------------|------------------------|
| (A) PROP. GROUND              | (E) AGGREGATE SUBGRADE |
| (B) PAVEMENT, HOT-MIX ASPHALT | (F) GUARDRAIL          |
| (C) SHOULDER, AGGREGATE       |                        |
| (D) SHOULDER, HOT-MIX ASPHALT |                        |

**PROPOSED TYPICAL SECTIONS**

**WALKER RD. OVER BURLINGTON CREEK**  
**KANE COUNTY**  
**SEC. 08-00133-01-BR**

**EXHIBIT 3-3**



**PROPOSED TYPICAL SECTION  
BRIDGE DECK**

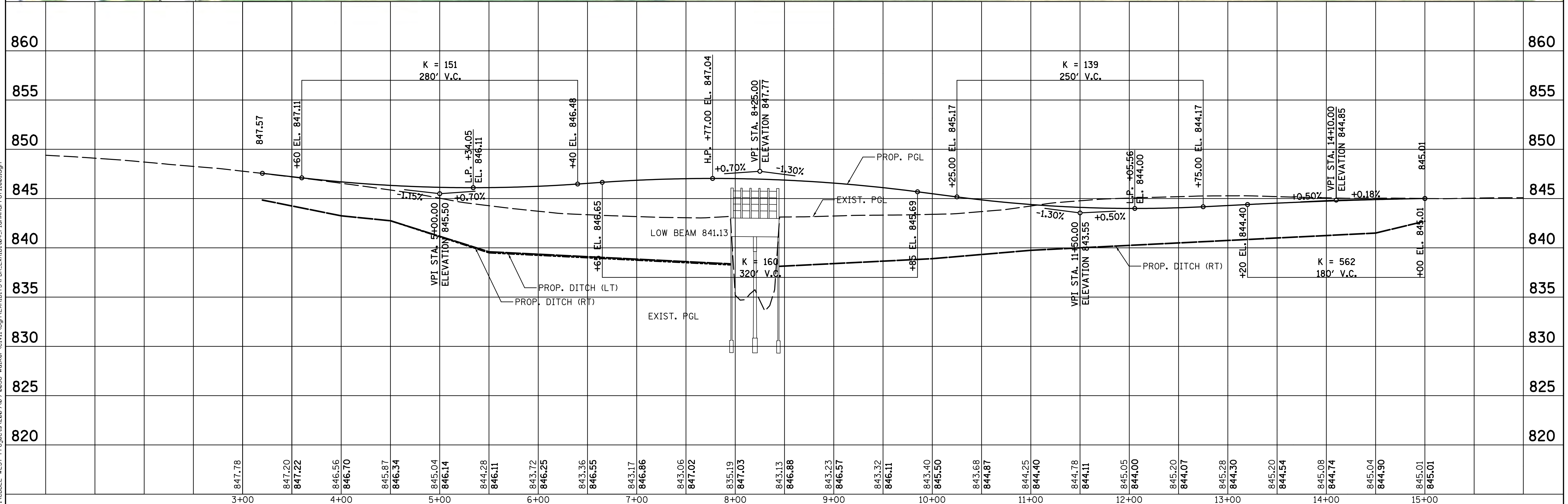
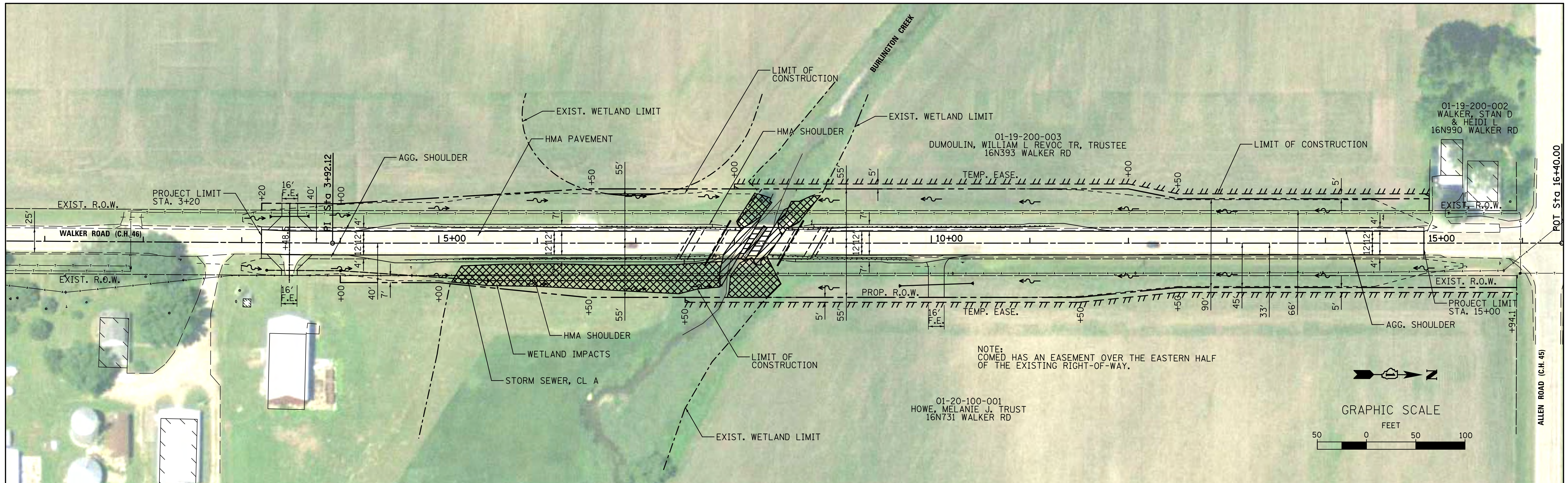
**LEGEND**

- (A) REINFORCED CONCRETE SLAB, 8"
- (B) WIDE-FLANGE STEEL BEAM (COMPOSITE)
- (C) TYPE SM STEEL RAILING

**PROPOSED TYPICAL SECTIONS**

**WALKER RD. OVER BURLINGTON CREEK  
KANE COUNTY  
SEC. 08-00133-01-BR**

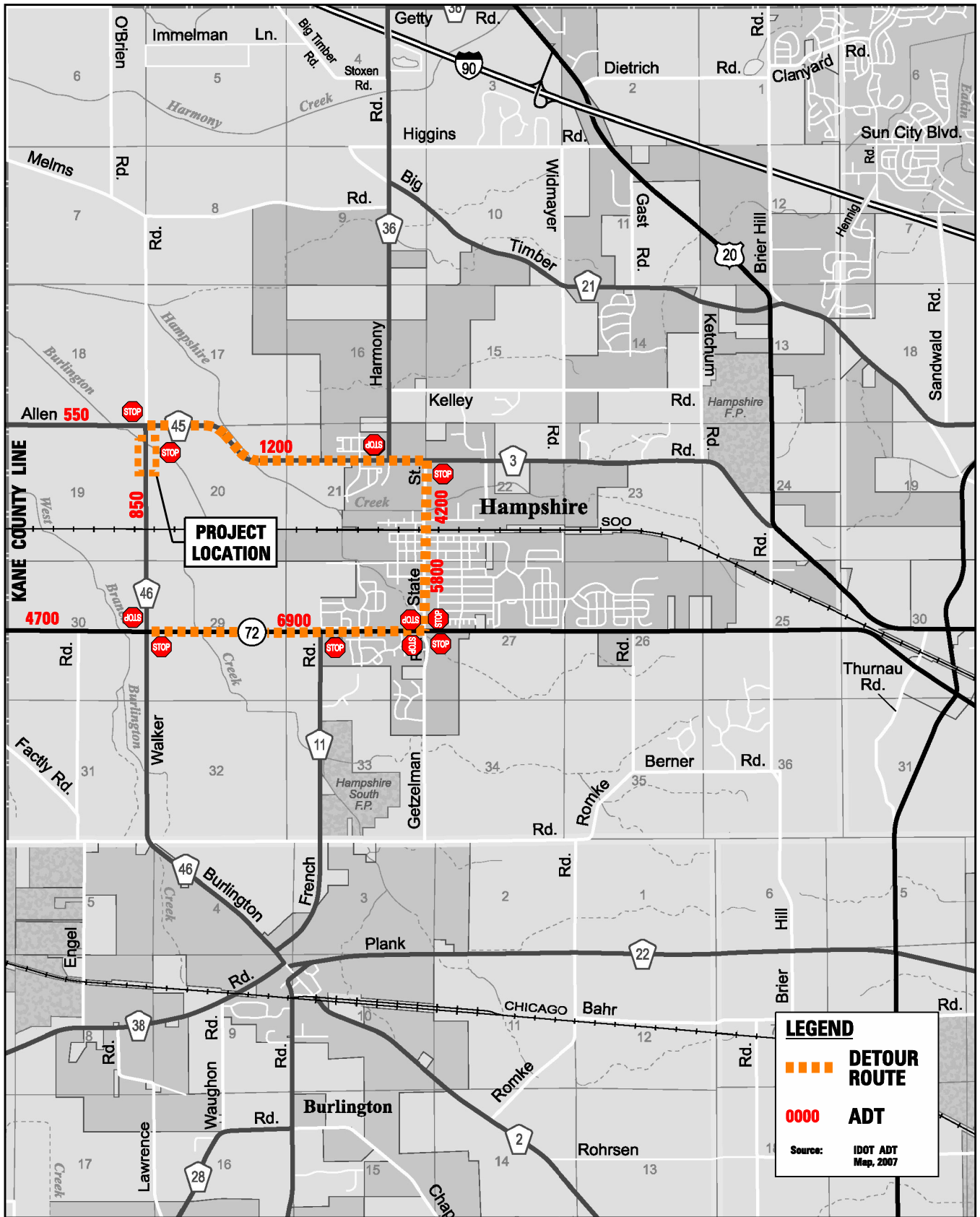
**EXHIBIT 3-4**



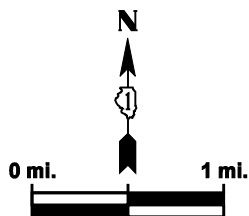
<b>WILLIS BURKE KELSEY ASSOCIATES LTD.</b> 116 West Main Street, Suite 201 St. Charles, Illinois 60174 (630) 442-7755	USER NAME = #USER#	DESIGNED - KMA	REVISED -
	PLOT SCALE =	DRAWN - KMA / NDP	REVISED -
	PLOT DATE = 4/5/2013	CHECKED - SBP	REVISED -
		DATE -	REVISED -
<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>			
<b>WALKER ROAD OVER BURLINGTON CREEK</b> <b>PLAN AND PROFILE</b>			
SCALE: 1"=50'		SHEET NO. 1 OF 1 SHEETS STA. 3+20 TO STA. 15+00	
C.H. 46	SECTION 08-00133-01-BR	COUNTY KANE	TOTAL SHEETS 1 SHEET NO. 1

**EXHIBIT 4**

FILE NAME = P:\CBBEL\_WEST Projects\2009\09-0056 Walker\Civil\Drawings\VPD\Exhibits\04\_PlanandProfile.dwg



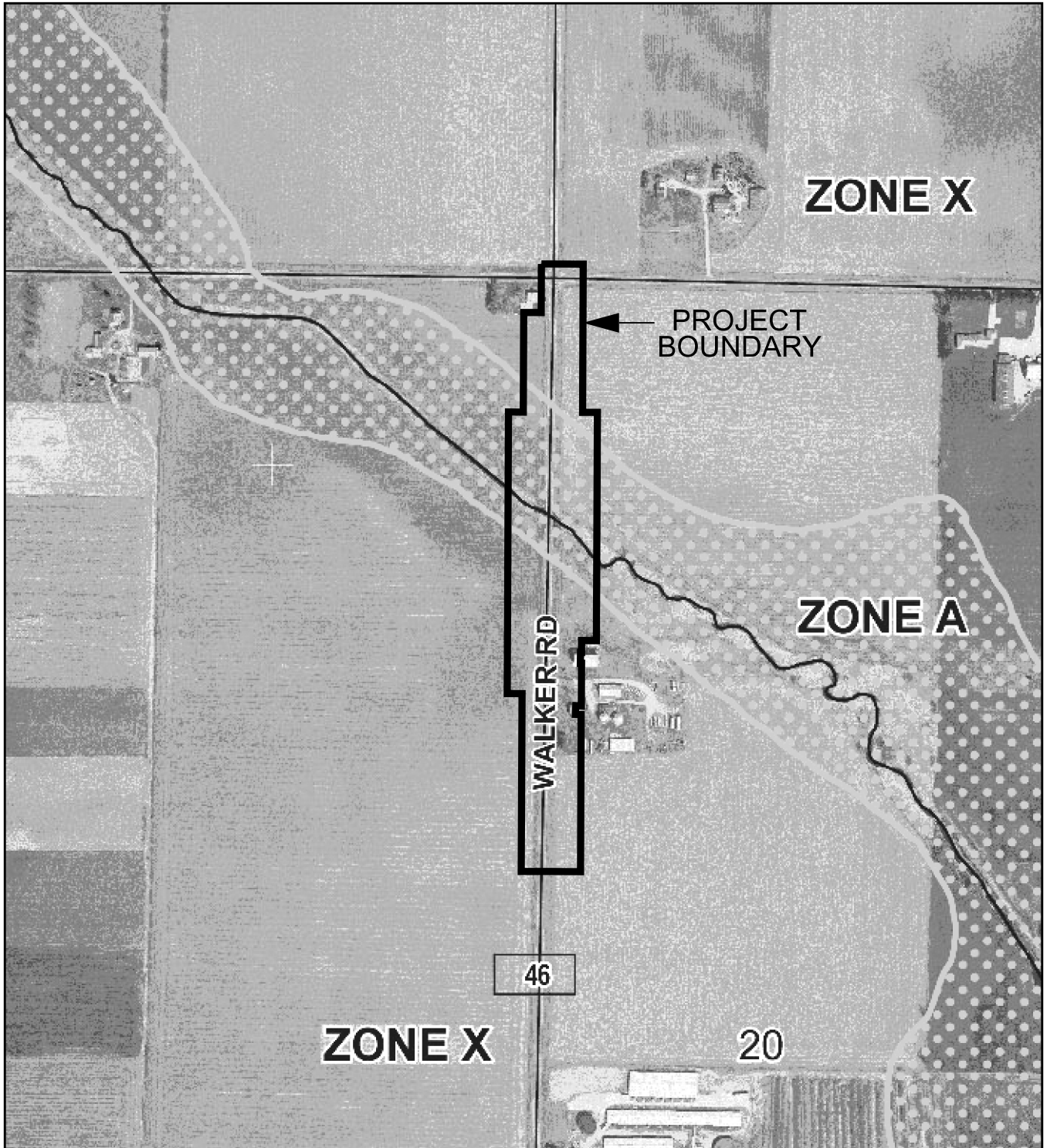
**WILLS BURKE KELSEY ASSOCIATES LTD.**  
**WBK**  
 116 West Main Street, Suite 201  
 St. Charles, Illinois 60174  
 (630) 443-7755



**DETOUR ROUTE MAP**

**WALKER RD. OVER BURLINGTON CREEK  
 KANE COUNTY  
 SEC. 08-00133-01-BR**

**EXHIBIT 5**

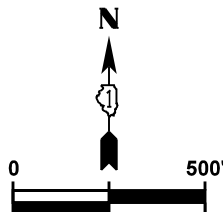


SOURCE(S): FEDERAL EMERGENCY MANAGEMENT, DIGITAL FLOOD INSURANCE RATE MAPS, AUGUST 3, 2009  
 Kane County, Illinois MAP NUMBER(S): 1708960105H

**LEGEND**

- ZONE A - No Base Flood Elevations Determined.
- ZONE X - Areas determined to be outside 500-year floodplain.

**WBK** WILLS BURKE KELSEY ASSOCIATES LTD.  
 116 West Main Street, Suite 201  
 St. Charles, Illinois 60174  
 (630) 443-7755



**FEMA FLOOD INSURANCE RATE MAP**

**WALKER RD. OVER BURLINGTON CREEK  
 KANE COUNTY  
 SEC. 08-00133-01-BR**

**EXHIBIT 6**

**ENGINEER'S OPINION OF PROBABLY CONSTRUCTION COSTS  
WALKER ROAD OVER BURLINGTON CREEK  
SECTION 08-00133-01-BR**

CODE	DESCRIPTION	UNIT	QUANTITY	UNIT COST	TOTAL COST
20200100	EARTH EXCAVATION	CU YD	3,145	\$20	\$62,900
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	1,870	\$22	\$41,140
20300100	CHANNEL EXCAVATION	CU YD	500	\$20	\$10,000
20400800	FURNISHED EXCAVATION	CU YD	1480	\$25	\$37,000
20800150	TRENCH BACKFILL	CU YD	40	\$30	\$1,200
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	3,880	\$2	\$7,760
21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	8,442	\$7	\$59,094
25000210	SEEDING, CLASS 2A	ACRE	2	\$2,000	\$4,000
28100107	STONE RIPRAP, CLASS A4	SQ YD	461	\$75	\$34,575
28200200	FILTER FABRIC	SQ YD	461	\$2	\$922
35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQ YD	120	\$12	\$1,440
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	180	\$26	\$4,680
35501318	HOT-MIX ASPHALT BASE COURSE, 8 1/2"	SQ YD	2,772	\$34	\$94,248
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	330	\$75	\$24,750
42001430	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	SQ YD	90	\$130	\$11,700
44000100	PAVEMENT REMOVAL	SQ YD	3,048	\$10	\$30,480
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	682	\$32	\$21,824
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1	\$50,000	\$50,000
50105220	PIPE CULVERT REMOVAL	FOOT	55	\$8	\$440
50200100	STRUCTURE EXCAVATION	CU YD	271	\$25	\$6,775
50300225	CONCRETE STRUCTURES	CU YD	62	\$650	\$40,300
50300255	CONCRETE SUPERSTRUCTURE	CU YD	202	\$750	\$151,500
50300260	BRIDGE DECK GROOVING	SQ YD	505	\$6	\$3,030
50300300	PROTECTIVE COAT	SQ YD	531	\$4	\$2,124
50500105	FURNISHING & ERECTING STRUCTURAL STEEL	L SUM	1	\$103,000	\$103,000
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	56,015	\$1.25	\$70,019
50901050	STEEL RAILING, TYPE SM	FOOT	211	\$135	\$28,485
51203200	FURNISHING METAL SHELL PILES 14 X 0.250	FOOT	600	\$55	\$33,000
51202305	DRIVING PILES	FOOT	600	\$1	\$600
51203200	TEST PILE METAL SHELLS	EACH	2	\$5,000	\$10,000
51500100	NAME PLATES	EACH	1	\$400	\$400
52100520	ANCHOR BOLTS, 1"	EACH	24	\$85	\$2,040
54213453	END SECTIONS 18"	EACH	1	\$500	\$500
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	2	\$570	\$1,140
54247100	GRATING FOR CONCRETE FLARED END SECTION 18"	EACH	2	\$260	\$520
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	400	\$44	\$17,600
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	58	\$25	\$1,450
60219000	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	3	\$2,200	\$6,600
63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	660	\$21	\$13,860
63100087	TRAFFIC BARRIER TERMINAL, TYPE 6A	EACH	4	\$3,200	\$12,800
63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	\$2,470	\$9,880
63200310	GUARDRAIL REMOVAL	FOOT	791	\$6	\$4,746
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	\$1,850	\$7,400
67100100	MOBILIZATION (4% of TOTAL)	L SUM	1	\$50,480	\$50,480
70101800	TRAFFIC CONTROL AND PROTECTION SPECIAL	L SUM	1	\$20,000	\$20,000
78008210	PAVEMENT MARKING TYPE I - LINE 4"	FOOT	4,720	\$1	\$4,720
X0326806	WASHOUT BASIN	L SUM	1	\$5,000	\$5,000
X2070304	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	106	\$35	\$3,710
Z0001050	AGGREGATE SUBGRADE 12"	SQ YD	3,038	\$13	\$39,494
	<b>Sub-Total</b>				<b>\$1,149,326</b>

**ENGINEER'S OPINION OF PROBABLY CONSTRUCTION COSTS  
WALKER ROAD OVER BURLINGTON CREEK  
SECTION 08-00133-01-BR**

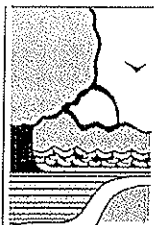
CODE	DESCRIPTION	UNIT	QUANTITY	UNIT COST	TOTAL COST
<b>Total from Page 1</b>					<b>\$1,149,326</b>
Z0013797	STABILIZED CONSTRUCTION ENTRANCE	SQ YD	110	\$12	\$1,320
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	\$15,000	\$15,000
Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	1060	\$30	\$31,800
Z0046304	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	105	\$23	\$2,415
<b>Sub-Total</b>					<b>\$1,199,861</b>
<b>Undeveloped Design Details (5%)</b>					<b>\$119,986</b>
<b>Utility Relocation (See Below)</b>					<b>\$70,000</b>
<b>TOTAL (Rounded)</b>					<b>\$1,390,000</b>

<b>ESTIMATE OF UTILITY RELOCATION COST</b>				
Relocate ComED Power Poles	EACH	6	\$10,000	\$60,000
Temporary De-Energizing of Overhead Power During Bridge Replacement Activities	L SUM	1	\$10,000	\$10,000
<b>UTILITY RELOCATION TOTAL</b>				<b>\$70,000</b>

**EXHIBIT 8**  
**SUMMARY OF RIGHT-OF-WAY AND EASEMENT ACQUISITION**

PARCEL NUMBER (PIN)	OWNER	PARCEL ADDRESS	USE / BLDG	PURPOSE	WIDTH OF TAKING	TEMPORARY EASEMENT		PERMANENT RIGHT-OF-WAY	
						SF	ACRES	SF	ACRES
					FT				
01-19-200-003	Dumoulin, William L, Trustee, Revoc. Trust	16N393 Walker Road, Hampshire IL	Farm	Accomdatation of Ditches and grading	ROW: 7' to 22' Temp. Ease: 5'	3,531	0.081	20,197	0.464
01-20-100-001	Howe, Melanie J. Trust	16N731 Walker Road, Hampshire IL	Farm and Buildings	Accomdatation of Ditches and grading	ROW: 7' to 22' Temp. Ease: 5'	4,221	0.097	19,705	0.452
<b>TOTAL</b>						<b>7,752</b>	<b>0.178</b>	<b>39,902</b>	<b>0.916</b>





# Illinois Department of Natural Resources

One Natural Resources Way Springfield, Illinois 62702-1271  
<http://dnr.state.il.us>

Pat Quinn, Governor  
Marc Miller, Director

Office of Water Resources • 2050 West Stearns Road • Bartlett, Illinois 60103

January 30, 2013

SUBJECT: Permit No. NE2013002  
Walker Road Bridge Replacement  
Burlington Creek  
Kane County, Application No. 2012189

RECEIVED

FEB 01 2013

Wills Burke Kelsey Associates

Carl Schoedel  
Kane County Division of Transportation  
41W011 Burlington Road  
St. Charles, Illinois 60175

Dear Mr. Schoedel:

Enclosed is Illinois Department of Natural Resources, Office of Water Resources Permit No. NE2013002 authorizing the subject project. This permit does not supersede any other federal, state or local authorizations that may be required for the project.

Please be advised that the Illinois Department of Natural Resources, Office of Realty and Environmental Planning (OREP) participates in the regulatory programs of the U.S. Army, Corps of Engineers (USACE) and may review this project if a USACE Section 10 or 404 permit is required. Issuance of a permit by the Office of Water Resources does not preclude OREP's provision of comments and/or recommendations, primarily related to biological effects of the proposed action, to the USACE and other federal agencies concerning your project.

If any changes of the permitted work are found necessary, revised plans should be submitted promptly to this office for review and approval. Also, this permit expires on the date indicated in Condition (13). If unable to complete the work by that date, the permittee may make a written request for a time extension.

Please contact me at 847/608-3100, ext. 32025 if you have any questions.

Sincerely,

Gary W. Jereb, P.E., Chief  
Northeastern Illinois Regulatory Programs Section

GJ:crw  
Enclosure

cc: Chicago District, U.S. Army Corps of Engineers  
John Witte, Wills Burke Kelsey Associates, Ltd. ✓  
Kane County Development Dept.

Exhibit 9



PERMIT NO. NE2013002  
DATE: January 30, 2013

**State of Illinois**  
Department of Natural Resources, Office of Water Resources

Permission is hereby granted to:

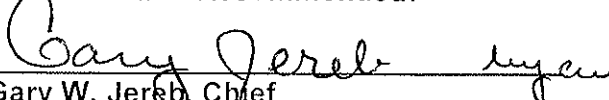
Kane County Division of Transportation  
41W011 Burlington Road  
St. Charles, Illinois 60175

to construct a replacement single-span bridge on Walker Road within the floodway of Burlington Creek in the Northeast Quarter of Section 19, Township 42 North, Range 6 East of the Third Principal Meridian in Kane County,


in accordance with an application dated November 15, 2012, and the plans and specifications entitled:

LOCATION MAP, WALKER ROAD OVER BURLINGTON CREEK, EXHIBIT 1, DATED SEPTEMBER 13, 2010, GENERAL PLAN AND ELEVATION, WALKER ROAD OVER BURLINGTON CREEK, KANE COUNTY, SHEET 1 OF 2, UNDATED, WALKER ROAD OVER BURLINGTON CREEK, PLAN AND PROFILE, EXHIBIT 4, UNDATED, ALL SHEETS RECEIVED NOVEMBER 8, 2012.

Examined and Recommended:

  
\_\_\_\_\_  
Gary W. Jereb, Chief  
Northeastern IL Regulatory  
Programs Section

Approval Recommended:

  
\_\_\_\_\_  
Arlan R. Juhl, Director  
Office of Water Resources

Approved:

  
\_\_\_\_\_  
Marc Miller, Director  
Department of Natural Resources

This PERMIT is subject to the terms and special conditions contained herein.

**THIS PERMIT IS SUBJECT TO THE FOLLOWING CONDITIONS:**

- 1) This permit is granted in accordance with the Rivers, Lakes and Streams Act "615 ILCS 5."
- 2) This permit does not convey title to the permittee or recognize title of the permittee to any submerged or other lands, and furthermore, does not convey, lease or provide any right or rights of occupancy or use of the public or private property on which the activity or any part thereof will be located, or otherwise grant to the permittee any right or interest in or to the property, whether the property is owned or possessed by the State of Illinois or by any private or public party or parties.
- 3) This permit does not release the permittee from liability for damage to persons or property resulting from the work covered by this permit, and does not authorize any injury to private property or invasion of private rights.
- 4) This permit does not relieve the permittee of the responsibility to obtain other federal, state or local authorizations required for the construction of the permitted activity; and if the permittee is required by law to obtain approvals from any federal or state agency to do the work, this permit is not effective until the federal and state approvals are obtained.
- 5) The permittee shall, at the permittee's own expense, remove all temporary piling, cofferdams, false work, and material incidental to the construction of the project. If the permittee fails to remove such structures or materials, the Department may have removal made at the expense of the permittee.
- 6) In public waters, if future need for public navigation or other public interest by the state or federal government necessitates changes in any part of the structure or structures, such changes shall be made by and at the expense of the permittee or the permittee's successors as required by the Department or other properly constituted agency, within sixty (60) days from receipt of written notice of the necessity from the Department or other agency, unless a longer period of time is specifically authorized.
- 7) The execution and details of the work authorized shall be subject to the review and approval of the Department. Department personnel shall have the right of access to accomplish this purpose.
- 8) Starting work on the activity authorized will be considered full acceptance by the permittee of the terms and conditions of the permit.
- 9) The Department in issuing this permit has relied upon the statements and representations made by the permittee; if any substantive statement or representation made by the permittee is found to be false, this permit will be revoked; and when revoked, all rights of the permittee under the permit are voided.
- 10) In public waters, the permittee and the permittee's successors shall make no claim whatsoever to any interest in any accretions caused by the activity.
- 11) In issuing this permit, the Department does not ensure the adequacy of the design or structural strength of the structure or improvement.
- 12) Noncompliance with the conditions of this permit will be considered grounds for revocation.
- 13) If the construction activity permitted is not completed on or before December 31, 2016 this permit shall cease and be null and void.

**APPENDIX A**  
**ENVIRONMENTAL COORDINATION**

## Project Overview

**Submittal Date:** 10/01/2010 **Sequence No:** 16137  
**District:** 1 **Requesting Agency:** Local Kane Co. DOT **Project No:**   
**Contract #:**  **Job No.:** P-91-272-09  
**Counties:** Kane  
**Route:** N/A **Marked:** C.H. 46  
**Street:** Walker Road **Section:** 08-00133-01-BR  
**Municipality(ies):** Unincorporated **Project Length:** 0.6437 km 0.4 miles  
**FromTo (At):** Burlington Creek  
**Quadrangle:** Hampshire **Township-Range-Section:** T42N R8E Sections 19 & 20  
**Survey Target Date:**  **Anticipated Design Appr.:** 09/15/2011 **Anticipated Processing:** CE  
**Funding:**  Federal  State  TBP  MFT  Local Non-MFT

**Consultant:**   
**PTB No.:**  **Item No.:**  **PTB Date:**  **Prequal Level:**

<b>Sequence No:</b> 16137	<b>Biological</b>	<b>Wetlands</b>	<b>Cultural</b>	<b>Special Waste</b>
<b>Entered By</b>	BDE	No	BDE	
<b>Cleared for DA</b>	11/4/2010	1/10/2013	7/6/2011	
<b>Cleared for Letting</b>	11/4/2010	1/10/2013	7/6/2011	
<b>Resubmittal</b>				
<b>ResubmittalCleared</b>				
<b>Section:</b> 08-00133-01-BR	<b>Job No.:</b> P-91-272-09			
<b>FromTo (At):</b> Burlington Creek				

Notice of Intent	Project Initiation Ltr to FHWA	Public Info Meeting(s)		Notice of Availability		Public Hearing	Draft	ROD/FONSI Approved
		1st	2nd	Draft	Final			

**Project Phase Comments:**

### Environmental Survey Request

**A. Project Information**

Bio  Cultural  Wetlands  Special Waste

Submittal Date: 10/01/2010 Sequence No: 16137  
District: 1 Requesting Agency: Local Kane Co. DOT Project No:   
Contract #: Job No.: P-1-272-09  
Counties: Kane  
Route: N/A Marked: C.H. 46  
Street: Walker Road Section: 08-00133-01-BR  
Municipality(ies): Unincorporated Project Length: 0.644 km 0.4 miles  
From To (At): Burlington Creek  
Quadrangle: Hampshire Township-Range-Section: T42N R8E Sections 19 & 20  
Anticipated Design Approval: 09/15/2011 **6E**

**B. Reason for Submittal: (Check all that apply)**

Acquisition of additional ROW or easement 0.6070275 ha/ 1.5 acres  
 In-Stream Work Stream Name: Burlington Creek  
 Other:

**C. Project Description:**

The project will replace the two lane bridge over Burlington Creek. The approach roadways will be improved from touchdown-to-touchdown. The embankment will be raised to provide the required freeboard between the low chord and the base flood elevation.

Proposed Work:  Highway  Bridge  Bike Trail  Other

**D. Tree Removal?:** Don't Know Number?: ha/ acres

Existing Bridge(s) Structure Number: 045-3036 On Historic Bridge List: No  
Historic District Involved? No Historic Buildings Involved? No  
Section 4(f) Lands Involved? No Section 6(f) Lands Involved? No  
Wetland delineation performed by: Consultant End. Species Consultation performed by: BDE

**E. Funding:**  Federal  State  TBP  MFT  Local Non-MFT

404 Permit Required Anticipated Processing: CE

**F. Contact Person:** Marilyn Solomon Local Contact Person: Mike Zakosek  
Telephone #: (847) 705-4407 ext. Telephone #: (630) 406-7346 ext.  
Env. Contact: E-Mail: zakosekmike@co.kane.il.us  
Telephone #: Title/Company:

Field Sign Off (Bio & Cultural Only)  Received In CO  SW Received

**BIOLOGICAL & WETLAND RESOURCES**  
**NO SURVEY OR FURTHER COORDINATION REQUIRED**  
Thomas C Busbo 11-4-10  
SIGNED *TMV* DATE

# Biological Resources

**Submittal Date:** 10/01/2010 **Sequence No:** 16137  
**District:** 1 **Requesting Agency:** Local Kane Co. DOT **Project No:** \_\_\_\_\_  
**Contract #:** \_\_\_\_\_ **Job No.:** P-91-272-09  
**Counties:** Kane  
**Route:** N/A **Marked:** C.H. 46  
**Street:** Walker Road **Section:** 08-00133-01-BR  
**Municipality(ies):** Unincorporated **Project Length:** 0.6437 km 0.4 miles  
**FromTo (At):** Burlington Creek  
**Quadrangle:** Hampshire **Township-Range-Section:** T42N R8E Sections 19 & 20  
**Anticipated Design Approval:** 09/15/2011 **Cleared for Design Approval:** 11/04/2010  
**Cleared for Letting:** 11/04/2010 **Anticipated Processing:** CE

**Acquisition of additional ROW or easement** 0.6070275 ha/ 1.5 acres  
**Tree Removal?:** Don't Know **Number?:** \_\_\_\_\_ ha/ \_\_\_\_\_ acres  
 **In-Stream Work** No **Class I Stream Involved:** \_\_\_\_\_  
**Wetland(s) Survey:** No  
**T&E Species:** No **Natural Areas:** No **Nature Preserves:** No  
**Prairie:** No **Railroad ROW:** \_\_\_\_\_ **Abandoned Railroad:** \_\_\_\_\_  
**Biological Sign Off:** 11/04/2010 **Field Sign Off:** \_\_\_\_\_ **District Sign Off:** \_\_\_\_\_  
**Wetland Sign Off:** 11/04/2010 **Surveys Performed:** \_\_\_\_\_ **Commitments:** \_\_\_\_\_

BRR										
District Notified	IDNR Notified	USFWS Notified	NPS Notified	IDNR Response		USFWS Response	NPS Response	District Notified		
				<input type="checkbox"/> Comments	<input type="checkbox"/> Concurrence			IDNR	USFW	NPS
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>			<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

ITA  Translocation

**Comments:** USFWS Section 7 Consultation- no wetlands or prairie in project corridor so no impacts to EPFO. That is t

**Endangered Species Consultation**

**NRRT (Natural Resources Review Tool)** 11/04/2010  
**EcoCAT (Ecological Compliance Assessment Tool)**

Submitted	Initial Consultation	Final Consultation	NRRT(OLD)
	<input type="checkbox"/> Terminated	<input type="checkbox"/> Terminated	
Resubmitted	Consultation Renewal	NRRT or EcoCAT:	
	<input type="checkbox"/> Terminated		

Biological Assessment							
IDNR Notified	USFWS Notified	IDNR Response	USFWS Response	District Notified		IDNR Consultation	USFWS Opinion
				IDNR	USFWS		
<input type="checkbox"/>	<input type="checkbox"/>			<input type="checkbox"/>	<input type="checkbox"/>		

**Comments:** No wetlands on NW1. Ag land. (JMV)

Further Studies	Federal Species *	Tasked	Report Due Date	Results Received
<b>Bio/Cover Type:</b>				
<b>Mammals:</b>	<input type="checkbox"/>			
<b>Birds:</b>	<input type="checkbox"/>			
<b>Plants:</b>	<input type="checkbox"/>			
<b>Herps:</b>	<input type="checkbox"/>			
<b>Fish:</b>	<input type="checkbox"/>			
<b>Mussels:</b>	<input type="checkbox"/>			
<b>Inverts:</b>	<input type="checkbox"/>			
<b>Other:</b>				

**Comments:** A-3

## Cultural Resources

**Submittal Date:** 10/01/2010 **Sequence No:** 16137   
**District:** 1 **Requesting Agency:** Local Kane Co. DOT **Project No:**   
**Contract #:**  **Job No.:** P-91-272-09  
**Counties:** Kane  
**Route:** N/A **Marked:** C.H. 46  
**Street:** Walker Road **Section:** 08-00133-01-BR  
**Municipalities:** Unincorporated **Project Length:** 0.6437 km 0.4 miles  
**From To (At):** Burlington Creek  
**Quadrangle:** Hampshire **Township-Range-Section:** T42N R8E Sections 19 & 20  
**Anticipated Design Approval:** 09/15/2011 **Cleared for Design Approval:** 07/06/2011  
**Cleared for Letting:** 07/06/2011 **Anticipated Processing:** CE

**Project Description:** The project will replace the two lane bridge over Burlington Creek. The approach roadways will be improved from touchdown-to-touchdown. The embankment will be raised to provide the required freeboard between the low chord and the base flood elevation.

**Funding:**  Federal  State  TBP  MFT  Local Non-MFT  
**Proposed Work:**  Highway  Bridge  Bike Trail  Other   
 Acquisition of additional ROW or easement 0.6070275 ha/ 1.5 acres

### Overall Cultural Resource

In-House Cultural Resources Clearance  District Notified:

### Archaeological Resources

In-House Archeology Only Clearance  District Notified: 07/06/2011

Sent for Survey ITARP: 10/26/2010 SHPO Concurrence: 06/01/2011

### Architectural Resources (Standing Structures)

In-House Architectural Only Clearance  District Notified:

Historic District Involved: No Historic Building Involved: No

Architectural Photos Requested:  Photos Received:

Sent for Architectural Survey:  SHPO Concurrence:

### Historic Bridges

In-House Historic Bridge Only Clearance  District Notified:

**Existing Bridge(s) Structure Number:** 045-3036 **On Historic Bridge List:** No

Sent for Archival Recordation:  Substitute Bridge Identification:

SHPO Submittal:

Comments:



# Wetlands

<b>Submittal Date:</b> 10/01/2010	<b>Sequence No.:</b> 16137	<b>District:</b> 1	<b>Requesting Agency:</b> Local Kane Co. DOT	<b>Project No.:</b>
<b>Contract #:</b>	<b>Job No.:</b> P-91-272-09	<b>Counties:</b> Kane		
<b>Route:</b> N/A	<b>Marked:</b> C.H. 46	<b>Street:</b> Walker Road		
<b>Municipality(ies):</b> Unincorporated	<b>Section:</b> 08-00133-01-BR	<b>Project Length:</b> 0.6437 km 0.4 miles		
<b>FromTo (At):</b> Burlington Creek	<b>Quadrangle:</b> Hampshire			
<b>Survey Target Date:</b>	<b>Anticipated Design Apprvl:</b> 09/15/2011	<b>Cleared for Design Apprvl:</b> 01/10/2013		
<b>Cleared for Letting:</b> 01/10/2013	<b>Mitigation:</b> Yes	<b>Mitigation Completed:</b>		

<b>Initial Survey and WIE</b>		<b>Addendum No.:</b>							
Initiated	Due Date	Results Received	Wetland Present	District Notified	WIE Requested	WIE Received	Wetland Impacts	Resp to District	Coord Complete
		10/09/2012	Yes			10/09/2012	Yes	01/10/2013	Yes

**Comments:**

**Clearances:** Cultural: 7/6/2011 Bio: 11/4/2010 SW:

**Processing** Programmatic Action

**Individual Compensation Plan Required:**

**404 Individual Permit Required:**

**Mitigation Site:** Wetland Bank Site

**Mitigation Basin:** In Basin

**Bank:**

**Accumulation:**

**Owner:**

**Name:**

**Location:**

**Size:**

**Types:**

**Quad:**

**Basin:**

**Processing Comments:**

**Wetland Impacts Evaluation**

<b>Submittal Date:</b> 09/24/2012	<b>Submitted By:</b>
<b>Does the project have wetland impacts?</b> Yes	<b>Type:</b> Both
<b>Briefly describe the measures considered to avoid and minimize adverse impacts to the wetlands:</b>	Construction will be limited to the Right-of-Way. Soil erosion and sediment control measures will be in place and maintained during construction to minimize wetland impacts off-site.
<b>Summarize briefly why there are no practicable alternatives to the use of the wetland(s):</b>	The pier is in poor condition, and Burlington Creek overtops the bridge during the 100-year flood event.
<b>Wetland mitigation is being proposed:</b> wetland bank site	<input checked="" type="checkbox"/> <b>Reviewed</b>

<b>Memo Date:</b> 01/10/2013	<b>Memo By:</b> Janel Veile
<b>Memo:</b> This office received the Wetland Assessment prepared by Wills Burke Kelsey Associates and the Wetland Impact Evaluation (WIE) form submitted for this project.	
Based on the information submitted for this project, there will be 0.245 acre of permanent impacts by fill and 0.009 acre of temporary impacts, resulting in a total of 0.254 acre of wetland impacts (Wetland Sites 1, 2, 3, 4, 5 and 6).	

Wetland Site 1 is a wet meadow located on the southwest side of the intersection of Walker Road and Burlington Creek. This wetland has an FQI of 2.4, indicating very low natural quality. An anticipated 0.014 acre of this 0.06 acre wetland will be permanently impacted.

Wetland Site 2 is a wet meadow located on the southeast side of the intersection of Walker Road and Burlington Creek. This wetland has an FQI of 5.1, indicating low natural quality. An anticipated 0.15 acre of this 0.67 acre wetland will be permanently impacted.

Wetland Site 3 is a wet meadow located on the northwest side of the intersection of Walker Road and Burlington Creek. This wetland has an FQI of 7.2, indicating low natural quality. An anticipated 0.015 acre of this 0.07 acre wetland will be permanently impacted.

Wetland Site 4 is a wet meadow located on the northeast side of the intersection of Walker Road and Burlington Creek. This wetland has an FQI of 4.2, indicating very low natural quality. An anticipated 0.037 acre of this 0.13 acre wetland will be permanently impacted.

Wetland Site 5 is an open water wetland. An anticipated 0.028 acre of this 0.14 acre wetland will be permanently impacted and 0.009 acre will be temporarily impacted. Please note that this section of Burlington Creek is recognized as a High Quality Stream according to the Kane County ADID map. As such, Kane County may require higher mitigation ratios. The highest mitigation ratio shall be applied.

Wetland Site 6 is a farmed wetland. An anticipated 0.001 acre of this 0.46 acre wetland will be impacted.

According to the information submitted for this project, mitigation has been proposed to occur at an in-basin wetland mitigation bank site. This office concurs with that form of mitigation. A mitigation ratio of 1.5:1.0 will be applied to the 0.245 acre of permanent wetland impacts and a ratio of 1.0:1.0 will be applied to the 0.009 acre of temporary wetland impacts, thus requiring a total of 0.3765 acre of credit to be purchased. Please note that credits must be purchased prior to construction according to the Implementing Rules of the IWPA.

With regards to wetlands, this project is clear for letting.

**Memo Date:** 09/24/2012      **Memo By:** Natalie Paver

**Memo:** Site 1 is Wetland 1, Site 2 is Wetland 2, Site 3 is Wetland 3, Site 4 is Wetland 4, Site 5 is Waters 1 (Burlington Creek), Site 6 is Farmed Wetland A. Site 5, Burlington Creek, has 0.028 acres of permanent impact, and 0.009 acres of temporary impact, for a total impact of 0.037 acres. The total permanent wetland impact for Sites 1, 2, 3, 4, and 6 is 0.218 acres. Site 5 (Waters 1) is rated a High Quality Stream according to the Kane County ADID map.

**Wetland Impacts and Mitigation Required**

Site No.	Type	T&E	Nature Preserve	Natural Area	Essential Habitat	Size (acres)	Acres of Impact	Ratio	Acres of Compensation
1	Wet Mead	No	No	No	No	0.06	.014	1.5	.021
<b>Basin</b>	07090006	<b>Quadrangle</b>	Hampshire		<b>FQI</b>	2.4			
<b>Describe the work:</b>		Fill							
2	Wet Mead	No	No	No	No	0.67	.150	1.5	.225
<b>Basin</b>	07090006	<b>Quadrangle</b>	Hampshire		<b>FQI</b>	5.1			
<b>Describe the work:</b>		Fill							
3	Wet Mead	No	No	No	No	0.07	.015	1.5	.023
<b>Basin</b>	07090006	<b>Quadrangle</b>	Hampshire		<b>FQI</b>	7.2			
<b>Describe the work:</b>		Fill							
4	Wet Mead	No	No	No	No	0.13	.037	1.5	.056
<b>Basin</b>	07090006	<b>Quadrangle</b>	Hampshire		<b>FQI</b>	4.2			
<b>Describe the work:</b>		Fill							
5	Open Water	No	No	No	Yes	0.14	.028	1.5	.042
<b>Basin</b>	07090006	<b>Quadrangle</b>	Hampshire		<b>FQI</b>	n/a			
<b>Describe the work:</b>		Fill							
5T	Open Water	No	No	No	Yes	0.14	.009	1.0	.009
<b>Basin</b>	07090006	<b>Quadrangle</b>	Hampshire		<b>FQI</b>	n/a			
<b>Describe the work:</b>									
6	Farmed	No	No	No	No	0.46	.001	1.5	.002
<b>Basin</b>	07090006	<b>Quadrangle</b>	Hampshire		<b>FQI</b>	n/a			
<b>Describe the work:</b>		Fill							
<b>Total</b>							.254		.377

Mitigation Site Suitability Study:

Wetland Compensation Plan:

Preparer:

Preparer:

Conceptual				
Plan Received	Agency	Report Sent and District Notified	Agency Response	District Notified
	IDNR			
	USFWS			
	COE			

Final				
Plan Received	Agency	Report Sent and District Notified	Agency Response	District Notified
	IDNR			
	USFWS			
	COE			

**Monitoring**

	Monitoring Reports			
	Received	COE Notified	IDNR Notified	District Notified
Year 1				
Year 2				
Year 3				
Year 4				
Year 5				

Monitoring Agency:

Construction Begin Date:

Construction Complete Date:

Tasked Date:

Monitoring Begin Date:

Monitoring Complete Date:

Monitoring Comments:

Permit(s) Type:  Corps Dist.:  Permit Issued:

Special Conditions:

Permit Agreements/Commitments:

**Project Phase**

Project Phase Comments:

**APPENDIX B**  
**STRUCTURAL AND HYDRAULIC APPROVALS**

**Illinois Department of Transportation  
Structures Information Management System  
Master Structure Report (S-107)**

Structure Number: 045-3036 District: 1

**Inventory Data**

Facility Carried:	WALKER ROAD	Bridge Name:		Sufficiency Rating:	25.7	Structure Length:	48.9
Feature Crossed:	BURLINGTON CREEK	Location:	1.4 MI N. ILL 72	HBP Eligible:	Yes	AASHTO Bridge Length:	46.4
Bridge Remarks:				Replaced By:	045-3065	Length of Long Span:	22.7
Bridge Status:	2 OPEN - LOAD POSTED	StatusDate:	01/2010	Replaces:		Bridge Roadway Width:	24.0
Status Remarks:				Last Update Date:	07/05/2012	Appr Roadway Width:	30.0
Maint County:	045 KANE	Maint Township:	10 HAMPSHIRE	Parallel Structure:	None	Deck Width:	28.5
Maint Responsibility:	03 COUNTY			Multi-Level Structure Nbr:		Sidewalk Width Right:	0.0
Service On/Under:	1 HIGHWAY	/	5 WATERWAY	Skew Direction:	Left	Sidewalk Width Left:	0.0
Reporting Agency:	3 COUNTY			Skew Angle:	35 D 0 M 0 S	Navigation Control:	0 No
Main Span Matl/Type:	1 CONCRETE	/	01 SLAB	Structure Flared:	No	Navigation Horiz Clear:	0
Nbr Of Main Spans:	2	Nbr Of Approach Spans:	0	Historical Significance:	No	Navigation Vert Clear:	0
***Approaches***				Border Bridge State:		Culvert Fill Depth:	0.0
Near #1 Matl/Type:		/		Bdr State SN:		Number Culvert Cells:	0
Near #2 Matl/Type:		/		Bdr State % Responsibility:	0	Culvert Opening Area:	0.0
Far #1 Matl/Type:		/		Structural Steel Wt:	0	Culvert Cell Height:	0.00
Far #2 Matl/Type:		/		Substructure Material:	55	Culvert Cell Width:	0.00
Median Width/Type:	0 Ft / 0 None			Rated By:	2 IDOT	Rate Method:	6
Guardrail Type L/R:	0 None / 0 None	Inventory Rating:	5.0 (29)	Load Rating Date:	08/31/2012	***Railroad Crossing Info***	
Toll Facility Indicator:	0 No Toll	Operating Rating:	18.0 (232)	Crossing 1 Nbr:		Crossing 1 Nbr:	
Latitude:	42 D 06 M 28.46 S	Longitude:	88 D 34 M 7.92 S	Design Load:	05 H15	RR Lateral Underclear:	.00
Deck Structure Type:	A CIP CON NRMLLY FORM	Deck Structure Thickness:	14.0	SD:	N	FO:	Y
Sidewalks Under Structure:	0 None	RR Vertical Underclear:	0 Ft 0 In				

**Key Route On Data**

Key Route Nbr:	COUNTY HIGHWAY	0046	Station:	.1500
Appurtenances	Main Route	00000	Segment:	
Inventory County:	045 KANE	Linked:	Y	
Township/Road Dist	10 HAMPSHIRE	Natl. Hwy System:	Not on NHS	
Municipality	0000	Inventory Direction:		
Urban Area:	None	Curr AADT Yr/Count:	2010 /   950	
Functional Class:	6	Est Truck Percentage:	10	
** CLEARANCES **	South/East	North/West	Number Of Lanes:	2
Max Rdwy Width:	23.8		One Or Two Way:	2 Two-Way
Horizontal:	25.8	.0	Bypass Length:	5
Min Vertical:	99 Ft 11 In	00 Ft 00 In	Future AADT Yr/Cnt:	2032 / 1212
10 Ft Vertical:	99 Ft 11 In	00 Ft 00 In	Designated Truck Rte:	NONE
Lateral:		Special Systems:	No	

**Key Route Under Data**

Station:		Station:	
Segment:		Segment:	
Linked:		Linked:	
Natl. Hwy System:		Natl. Hwy System:	
Inventory Direction:		Inventory Direction:	
Curr AADT Yr/Count:	/ /	Curr AADT Yr/Count:	/ /
Est Truck Percentage:		Est Truck Percentage:	
Number Of Lanes:		Number Of Lanes:	
One Or Two Way:		One Or Two Way:	
Bypass Length:		Bypass Length:	
Future AADT Yr/Cnt:	/ /	Future AADT Yr/Cnt:	/ /
Designated Truck Rte:		Designated Truck Rte:	
Special Systems:		Special Systems:	

\*\*\* Marked Route On Data \*\*\*

	Designation	Kind	Number
Route #1:	1 Mainline	4 FAS, CH, or TR's Unmarked	
Route #2:	1 Mainline		
Route #3:	1 Mainline		B-1

\*\*\* Marked Route Under Data \*\*\*

	Designation	Kind	Number

**Illinois Department of Transportation  
Structures Information Management System  
Master Structure Report (S-107)**

Date: 1/21/2013  
Page 2

Structure Number: 045-3036 District: 1

**Data Related to Inspection Information**

<b>***Inspection Intervals***</b>		<b>*** Maximum Allowable Posting Limits ***</b>				<b>Bridge Posting Level:</b>			
Routine NBIS:	24 MOS	Underwater:	0 MOS	One Truck At A Time:	0	Combination Type 3S-1:	29 Tons	4	< 10% Below Legal Loads
Fracture Critical:	0 MOS	Special:	N	Single Unit Vehicles:	22 Tons	Combination Type 3S-2:	36 Tons		

**Inspection/Appraisal Information**

<b>Inspection Date:</b>	01/10/2012	<b>Inspection Temperature:</b>	45 Deg. F	<b>Insp by (Name):</b>	C. SCHRAMM / HLR 2	<b>** Actual Posted Limits **</b>
<b>Deck:</b>	5	FAIR CONDITION - MINOR SECTION LOSS, CRACKS		<b>Insp by (Name):</b>		<b>Single Unit Vehicles:</b> 22 Tons
<b>Superstructure:</b>	5	FAIR CONDITION - MINOR SECTION LOSS, CRACKS		<b>Utilities Attached:</b>	0 STREAM GAUGE CO	<b>Combination Type 3S-1:</b> 29 Tons
<b>Substructure:</b>	5	FAIR CONDITION - MINOR SECTION LOSS, CRACKS			N N/A	<b>Combination Type 3S-2:</b> 36 Tons
<b>Culvert:</b>	N	NOT APPLICABLE			N N/A	<b>One Truck At A Time:</b> 0
<b>Channel and Protection:</b>	7	GOOD CONDITION - SOME MINOR PROBLEMS		<b>Deck Wearing Surf:</b>	G BITUMINOUS OVERLAY	<b>Last Paint Type:</b>
<b>Structural Evaluation:</b>	3	INTOLERABLE - HIGH PRIORITY FOR CORRECTION		<b>Deck Membrane:</b>	F NONE	
<b>Deck Geometry:</b>	4	MINIMUM ADEQUACY TO BE LEFT IN PLACE		<b>Deck Protection:</b>	J NONE	
<b>Underclearance-Vert/Lat.:</b>	N	NOT APPLICABLE		<b>Total Deck Thick:</b>	22.0	
<b>Waterway Adequacy:</b>	5	BETTER THAN ADEQUATE TO BE LEFT IN PLACE		<b>Last Paint Date:</b>		
<b>Approach Roadway Align:</b>	8	EQUAL TO PRESENT DESIRABLE CRITERIA		<b>Inspection Remarks:</b>	EXTENSIVE MAP CRACKING AND LEACHING ON THE UNDERSIDE OF SLAB WITHIN 5' OF FASCI AS. SPALLING WITH EXPOSED MAIN REBAR ALONG EAST & WEST FASCIA IN NORTH SPAN. FULL HEIGHT HAIRLINE & OPEN VERTICAL CRACKS WITH LEACHING ON SUBSTRUCTURE UNIT	
<b>Bridge Railing Appraisal:</b>	2	Doesn't Meet Standards				
<b>Approach Guardrail:</b>	333	Acceptable	Acceptable	Acceptable		
<b>Pier Navig Protection:</b>	N	N/A				

**Underwater Inspection/Appraisal Information**

<b>Inspection Date:</b>		<b>Inspection Category:</b>				
<b>Temperature:</b>		<b>Inspection Method:</b>				
<b>Inspected By:</b>		<b>Inspected By:</b>		<b>Appraisal Rating:</b>		
<b>Inspection Remarks:</b>						

**Scour Critical Information**

<b>Rating:</b> 5	CALCULATED SCOUR ACCEPTABLE	<b>Evaluation Method:</b> B	Rational Analysis
<b>Analysis Date:</b> 09/25/1996		<b>Analysis By:</b>	

**Miscellaneous**

<b>Fracture Critical Members:</b>	No
<b>Microfilm Data Recorded:</b>	No

**Construction Information**

<b>Year:</b> 1946	Original		Reconstructed
<b>Route:</b> SA 46	<b>Sta:</b> 8+20		<b>Sta:</b>
<b>Section Nbr:</b> 133B-MFT			
<b>Contract Nbr:</b>			
<b>Fed Aid Pr #:</b> 00000000000000			
<b>Built By:</b> 3	COUNTY AGENCY		

**Waterway Information**

<b>Flood Design Frequency:</b>		YRS	<b>Drainage Area:</b>		Acre
<b>Flood Design Q (CFS):</b>			<b>Flood Base Q (CFS):</b>		
<b>Flood Design Nat H W E:</b>			<b>Flood Base Nat H W E:</b>		
<b>Flood Des Open Prop:</b>		SF			

**Proposed Improvement**

<b>Cost Estimate Year:</b>		<b>Length:</b>		<b>*** Costs in Dollars ***</b>
<b>Type of Work:</b>				<b>Bridge Cost:</b>
<b>Done By:</b>				<b>Roadway Cost:</b>
<b>Remarks:</b>	B-2			<b>Total Project Cost:</b>

TC



# Illinois Department of Transportation

## Memorandum

---

To: Diane M. O'Keefe, District 1      Attn: Christopher J. Holt  
 From: Ralph E. Anderson              By: Carl Puzey  
 Subject: BRIDGE CONDITION REPORT APPROVAL  
 Date: November 15, 2010

---

*Carl Puzey*

Kane County  
 Section 08-00133-01-BR  
 Walker Road (CH 46) over Burlington Creek

SN 045-3036

We have finished a review of the Bridge Condition Report (BCR) for the above-designated project. We concur with the consultant's recommendation for total replacement of the existing bridge, and the BCR is hereby approved.

Please note that approval of the project is contingent on approval by others of the proposed geometry, obtaining environmental signoffs, and any required historic structure coordination and other approvals required by statutes or the policies of the Department.

If you have any questions, contact Jim Klein at 217/782-5928 or Tom Cartmel at 217/782-5929.

TC/kkt0453036-20101115



# Illinois Department of Transportation

## Memorandum

To: John A. Fortmann, District 1      Attn: Christopher J. Holt  
 From: D. Carl Puzey      By: Timothy A. Armbrrecht  
 Subject: PRELIMINARY BRIDGE DESIGN APPROVAL  
 Date: October 24, 2012

*Christopher J. Holt*  
*Timothy A. Armbrrecht*

RECEIVED

OCT 29 2012

BUREAU OF LOCAL  
ROADS & STREETS

Highway Bridge Program  
 Kane County  
 Section 08-00133-01-BR

SN 045-3065

C.H. 46 (Walker Road) over Burlington Creek

The preliminary bridge design and hydraulic report, dated September 21, 2012, for the above-designated bridge replacement project is hereby approved. This approval is also based on the hydraulics being approved by your office or by the Bartlett IDNR Office of Water Resources.

The Scour Critical Evaluation Coding Report was submitted and is acceptable based on modification to Item 113 being coded "8". This coding is appropriate for single span pile bent structures with rip rap armored embankments. This coding is also consistent with BLRS Circular Letter 2009-07, "Bridge Scour Supplement", issued March 31, 2009.

The PBDHR indicates the County would like to have the Department perform the steel fabrication inspection. Please advise the County a letter requesting fabrication inspection should be sent to the Bureau of Bridges and Structures (BBS) as soon as possible after the steel fabricator is determined. The letter (see attached example) should include the following:

1. Job information (structure number, route, section, county, IDOT contract # if applicable, C-# if applicable).
2. Point of contact for questions and who to send reports to at job completion; name, contact and location for fabricator and prime contractor.
3. The approximate start date and duration if known.

This request essentially authorizes the BBS' shop drawing reviewer and/or fabrication inspector to act as the County's representative. The inspector will need a copy of the shop drawings, approved by either this office or the County's consultant. If the shop drawings were not reviewed by the BBS, a second copy should also be provided to the BBS for office use in assisting the inspector with technical or interpretation questions. The inspector and this office will also require reference copies of any special provisions or project-specific specifications applicable to fabrication that are different from IDOT's Standard & Supplemental Specifications.



Mr. John A. Fortmann  
Page 2  
October 24, 2012

Please be aware fabrication inspection services supplied by the Department are subject to resource availability and are not guaranteed. In particular, if the fabricator is located outside the area served by Department inspectors, it may be necessary for the County to retain the services of their own fabrication inspection service to ensure the inspection of the steel.

The final structural plans and specifications for this project may be accepted by the District based on the Structural Engineer's seal, certification and signature per BLRS Manual Section 23-7.02.1. If you have any questions, contact Matt Humke at 217/782-5929 or [matt.humke@illinois.gov](mailto:matt.humke@illinois.gov).

Three copies of the approved report are being returned to you and we will retain one copy for our files.

MDH/kkt0453065-20121024

# LIVINGSTON COUNTY HIGHWAY DEPARTMENT

David R. Winters, P.E., County Engineer  
Clay A. Metcalf, P.E., Asst. County Engineer

1705 S. Manlove Street  
Pontiac, Illinois 61764

Ph. (815) 842-1184  
FAX (815) 842-3305

April 9, 2002

Major Bridge Program  
Pontiac Road District  
"Heisner's Island Bridges"

Structure No.: 053-4146  
District: 3  
Section: 93-22136-00-BR  
Contract No.: 87132  
Job No.: C-93-131-95  
Project No.: BRM-5056(7)  
Route: ~~FALC 506~~

ILLINOIS DEPARTMENT OF TRANSPORTATION  
BUREAU OF BRIDGES  
AND STRUCTURES

APR 11 2002

TJD REA DISC  
TEA ~~BRG~~ CIRC  
JAM POST

~~Mr. John A. Morris, Structural Services Section~~  
Illinois Department of Transportation  
Bureau of Bridges and Structures, Room 240  
2300 South Dirksen Parkway  
Springfield, Illinois 62764  
Attn: Fabrication Unit  
Gentlemen:

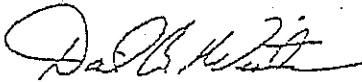
We formally request that the Illinois Department of Transportation perform shop inspection of the steel structures to be fabricated for the above captioned project. This is in accordance with Chapter 5, Section 5(c) of the Federal Aid Procedures for Local Highway Improvements manual. The prime contractor and fabricator information are as follows:

Prime Contractor  
H.J. Eppel & Co., Inc.  
1400 Tuesburg Court  
Pontiac, IL 61764  
Mark Eppel (815)844-7269

Fabricator  
Delong Inc.  
P.O. Box 479  
Jefferson City, MO 65102-0479  
Darrin B. Kelly (573)635-6121

During the preconstruction meeting it was stated that structural steel is scheduled to be delivered in September of 2002. If you have any questions concerning this submittal, please do not hesitate to call and ask for Clay Metcalf.

Very truly yours,



David R. Winters  
County Engineer

DRW:cam

CC: IDOT - District 3  
H.J. Eppel & Co., Inc.  
Delongs Inc.

Z:\17WPWork\Pontiac 22\Bridges\22136br-Heisner Island\CONSTRUCTION\Fabrication\inspection-IDOT.doc

**APPENDIX C**  
**PERMIT COORDINATION**



## **MEETING NOTES**

Date: December 5, 2011

By: Natalie Paver

Attendees: Kimberly Kubiak, USACE Chicago District  
Natalie Paver, WBK

Copies To: File

Subject: Walker Road over Burlington Creek Wetlands (WBK #09-0056)

Meeting Date: October 18, 2010

Location: On-site

---

An on-site meeting was held on October 18, 2010 to verify the limits of the wetland delineation. The wetland delineation was performed on August 17, 2010 by Wills Burke Kelsey Associates, Ltd (WBK). Items discussed at the meeting included the wetland indicator criteria for vegetation, soils, and hydrology. The original delineation included fringe wetlands on each side of the Walker Road and Burlington Creek bridge crossing, and a larger wetland on the southeast side of the crossing. Also, one ditch was delineated along Walker Road. The ditch was located on the north east side of the road and creek crossing.

Kim Kubiak confirmed the wetland delineation limits along the creek and in the wetland on the southeast side of the road and bridge crossing. She did not accept the delineation limits of the ditch along the road. WBK agreed that this area should not be considered wetland, and removed the ditches from the wetland delineation exhibits and report.

Potential farmed wetland areas were also discussed. Kim suggested that the field on the southwest side of the bridge crossing should be checked for farmed wetland by completing a farmed wetland assessment. A portion of this field is considered NRCS Farmed Wetland on the Kane County ADID map.

Kim Kubiak assigned the project to LRC # 2010-637.

The above notes do not constitute minutes from the meeting, they are only notes of various topics that were discussed and may not be all-inclusive. If there are any discrepancies, please contact Wills Burke Kelsey Associates, Ltd.

**APPENDIX D**  
**FHWA COORDINATION**



# Meeting Minutes

Wills Burke Kelsey Associates, Ltd.  
Consulting Engineers

116 West Main Street  
Suite 201  
St. Charles, IL 60174

Project	<u>Walker Road over Burlington Creek</u>	Meeting Date	<u>September 11, 2012</u>
Client/Agency	<u>Kane County Division of Transportation</u>	Meeting Time	<u>9:00 A.M.</u>
IDOT Section #	<u>08-00133-01-BR</u>	Meeting Location	<u>IDOT D1</u>
IDOT Job #	<u>P-91-272-09</u>	Meeting Organizer	<u>Andy Underwager</u>
IDOT Project #	<u>BROS-0089(155)</u>	Meeting Topic	<u>IDOT/FHWA Coord.</u>
WBK Project #	<u>R03.090056.00000</u>		<u>1<sup>st</sup> Presentation</u>

### Purpose of Meeting: Concurrence on scope, geometry, termini, and CE II processing

1. Introductions/ Attendees: See Attendance Roster
2. General comments made by Andy Underwager (WBK):
  - Existing structure is located on Walker Road approximately 1.4 miles north of Illinois Route 72 in Hampshire Township. Walker Road is designated County Highway 46 and has a functional classification of Minor Collector, (Non-Urban).
  - Bridge originally built in 1946 and is not a historic structure.
  - The sufficiency ratio is 32.8, making it eligible for replacement under the HBP.
  - The BCR has been approved (11/15/2010)
  - The scope of work is bridge replacement with profile adjustment necessary to accommodate hydraulic capacity and 1' freeboard.
  - Traffic to be detoured during construction. State Route 72 proposed. Detour report submitted and approved. Further coordination in Phase II.
  - ESR has been submitted; Cultural, Biological and Wetland Clearances received.
  - Wetlands delineated and WIE submitted 9/7/2012 subsequent to Wetland Clearance.
  - Wetland impacts are anticipated and will be mitigated at a wetland bank.
  - Compensatory storage is required and anticipated to be provided by ditches constructed along the roadway.
  - Right of way and/or easements needed from 2 different parcels.
  - Burlington Creek is a non-regulatory floodway, Zone A. A formal permit is required from IDNR-OWR.

### 3. Discussion:

#### **Scope:**

The scope of work is bridge replacement and profile adjustment. The BCR has been submitted and approved for bridge replacement.

"Design Criteria for New or Reconstructed Rural Bridges", Figure 36-5A were utilized. The bridge will be sized to accommodate the 30-yr flood frequency, provide 1-foot of freeboard to the low chord and rip-rap armored slope walls to prevent the potential for scour. Based on the projected DHV of 6,000 (2040) the new bridge will incorporate 2-12' lanes, 4' HMA shoulders and Type SM steel bridge railing measuring 32'-0" out to out. The bridge is to be built on a crest vertical curve alignment and tangent horizontal alignment.



### **Roadway Geometry:**

“Geometric Design Criteria for Rural Two-Lane Collectors”, Figure 32-2A were utilized to develop the proposed geometry. The proposed cross-section consists of 2-12’ lanes, and 4’ HMA shoulders.

Guardrail will be provided in areas where side slopes are greater than 1V:4H.

Bicycle trails or sidewalks do not currently exist along Walker Road and there are no plans to include bicycle accommodations.

**The FHWA concurred on the scope of work and proposed geometry, but noted the PBDHR would need to be approved.**

### **Termini:**

The project termini were established to provide a smooth transition with the existing pavement elevations based on profile adjustment. The proposed project limits extend from Sta. 3+20 to Sta. 15+00 (1,180 LF).

**The FHWA concurred on the logical termini.**

### **CE II Processing:**

Due to the feature crossed and impacts to adjacent wetlands it was discussed to process the project as a Categorical Exclusion II.

**The FHWA concurred to process this project as a CE II.**

### **Public Involvement/Land Acquisition:**

Right of way/easements are anticipated from 2 parcels. Since this is less than 10 parcels, the consultant proposed to publish a notice for the opportunity for a public meeting. The IDOT/FHWA suggested sending a letter to the parcel owners outlining the project, the amount of right-of-way and/or easement needed and the appraisal and negotiation process. WBK agreed to this.

**The FHWA concurred on the public involvement process of the project.**

### **Design Variances:**

The consultant identified the following design exceptions:

- Shoulder Widths: 4’ HMA shoulder widths are proposed along the length of the project to minimize the amount of compensatory storage and wetland impacts. The existing shoulders are less than 4 feet wide within the project limits and for the adjacent segments of Walker Road. Per BLRS design criteria, 8’ is the required minimum. **FHWA concurred.**



## Meeting Minutes

---

- Side Slopes: 1V:3H and steeper side slopes are proposed along Walker Road behind the guardrail to transition the steeper 1V:2H slopes near the bridge. 1V:4H side slopes for fill sections are required. **FHWA concurred.**

#### 4. Action Items:

- Submit PBDHR
- Initiate public involvement process
- Submit Draft PDR after approval of PBDHR.

*The above notes constitute minutes from the meeting. If there are any discrepancies, please contact Wills Burke Kelsey Associates, Ltd. within 3 business days.*





**Illinois Department  
of Transportation**

**FHWA/Local Coordination Meeting  
Attendance Roster**

<b>Agency Name:</b>	Kane County		
<b>Project &amp; Topic:</b>	Walker Road over Burlington Creek; Bridge Rehab: Scope, logical termini, processing		
<b>Section No.:</b>	08-00133-01-BR		
<b>Date:</b>	September 11, 2012	<b>Time:</b>	9:00 AM
<b>Location:</b>	Region/District One	<b>Room:</b>	Executive Conference Room

	<b>NAME (Please Print)</b>	<b>REPRESENTING</b>	<b>PHONE NUMBER</b>
1	<del>Chris Holt</del>	IDOT BLRS	(847) 705-4201
2	Salmon Danmole, SD	IDOT CBLRS	(217) 785-2798
3	<del>Dennis Bachman</del>	FHWA	(217) 492-4283
4	<del>Chris Byers</del>	FHWA	(312) 886-1606
5	<del>Robin Helmerichs</del>	FHWA	(217) 492-4615
6	Michael Hine MH	FHWA	(217) 492-4634
7	Ehad Riddle	IDOT-BLRS	847 705.4406
8	JOY GUSTAFSON	IDOT DL/LAND Acq	847 705 4296
9	MIKE ZAKOGEK	KANE CO. DOT	630-584-1170
10	Andy Underwagner	WBK	630 443 7755
11	MARILYN SOLOMON	IDOT-DL-BLRS	847-705-4407
12	SULEYMAN TULGAR	" " " (HE GREEN)	847 - 705 - 4205
13	Mike Sullivan	KKCOM	630-444-3142
14			
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**APPENDIX E**  
**DETOUR APPROVAL**



# Illinois Department of Transportation

201 West Center court  
Schaumburg, IL 60196-1096

Informal Transmittal

*CAJ Consult. Copy*

*S.P.*

To:	Cory Jucius
Bureau:	Traffic
Attn:	

From:	Chris Holt
Bureau:	Local Roads & Streets - Dist. 1
By:	Marilyn Solomon
Subject:	Kane County*
	Section No.: 08-00133-01-BR

Date:	11/12/10
-------	----------

Please check appropriate box below:

- Take Necessary Action
- For Your Comments
- Per Your Request
- For Your Approval

- For Your Information
- See Me About the Attached
- Draft (Letter)(Memo) For My signature

- Reply
- Return
- Route
- File

## Message

\*Walker Road over Burlington Creek

Cory,

Attached are 2 copies of Detour Report for the subject federal aid project. County is proposing to utilize a portion of Illinois Route 72.

Please review and provide your comments / approval.

If you have any questions or need additional information please contact Marilyn Solomon at X-

**RECEIVED**

**NOV 18 2010**

**BUREAU OF LOCAL  
ROADS & STREETS**

*Marilyn Solomon/ST*  
Signature

Copies to	File		
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### Response

*① No objection to detour route. ② See comments to detour plan sheet. ③ Be Check w/ Programming to be sure IDOT has no proposed projects scheduled on IL 72.*

*Cory Jucius* 11/16/10  
Signature x4411

transmem - Revised 2/19/98



**Engineering  
Enterprises,  
Inc.**

52 Wheeler Road • Sugar Grove, IL 60554

TEL: 630 / 466-6700

FAX: 630 / 466-6701

www.eeiweb.com

October 28, 2010

Mr. S. Brent Potteroff  
Senior Project Manager  
WBK Associates  
116 West Main Street  
St. Charles, IL 60174

**Re: 09-0056 - Walker Road Bridge over Burlington Creek  
Village of Hampshire, Kane County, Illinois**

Mr. Potterhoff:

We are in receipt of your letter dated October 4, 2010 notifying us of the proposed detour. At this time, we have no objections to the proposed route.

If you have any questions or require additional information please contact our office.

Respectfully submitted,

ENGINEERING ENTERPRISES, INC.

Bradley P. Sanderson, P.E.  
Vice President

BPS/arf

pc: Mr. Jeff Magnussen, Village President  
Mr. Marty Ebert, Village Trustee  
Ms. Linda Vasquez, Village Clerk

RECEIVED

OCT 29 2010

Willis Burke Kelsey Associates

**APPENDIX F**  
**UTILITY COORDINATION**

Thank you for contacting JULIE, Inc. regarding your upcoming digging project.

Please review and print your locate request ticket below for your records. If any of the information is incorrect, please contact a JULIE call center agent by simply dialing 811 or 800-892-0123 and refer to the locate request number. The agents are available 24/7.

For information about the next steps in the process, a copy of JULIE's Homeowner's Guide, and an explanation of the color-code markings, visit [www.illinois1call.com/e\\_request/what\\_happens\\_next.htm](http://www.illinois1call.com/e_request/what_happens_next.htm)

Dig No : A1801496 Rev : 00A Digstart: 12/29/10 13:44  
Rcvd : 06/29/10 13:45 Priority: 2 Expires : 01/01/00 00:00  
Org Dig: A1801496 Rcvd: 06/29/10 13:43

Firm : WILLS BURKE KELSEY Caller: ANDREW HAGLUND  
CoAddr1: 116 W. MAIN ST., STE. 201  
City,St: ST. CHARLES, IL Zip : 60174  
Phone : 630-443-7755 Ext : Fax: 630-443-0533  
Call Bk: Done For : KANE CO DIVISION OF TRANS.  
SiteCnt: SAME  
Email : AHGLUND@WBKENGINEERING.COM

County : KANE Place: HAMPSHIRE  
Address:  
Subdiv : Cross:

Grids : T42NR06E19\*\* T42NR06E20\*\*

BestFit: 42.110025/-88.588284 42.110025/-88.549480  
: 42.095515/-88.588284 42.095515/-88.549480  
PreMark: NO Directional Boring: NO Depth>7Ft: YES  
Locatn : IN THE TOWNSHIP OF HAMPSHIRE, \*\* APPROX 2 MILES WEST OF HAMPSHIRE  
WrkType: DESIGN STAGE/REPLACE BRIDGE, ROAD WAY APPROCHES  
Extent : WORK WILL BE DONE SECTIONS AND QUARTERS  
Remarks:

Members:  
ATT51A ATT/DISTRIBUTION 630-573-5450x2nd 6  
305735495  
CEC00A COMED / JOLIET DESIGN STAGE LOCATE LINE 630-576-7094  
KCDTOA KANE COUNTY DIV. OF TRANSPORT KURT NIKA 630-406-7372  
NICR2A NICOR GAS UTILITY CONSULTANT G03W 630-388-2362  
SMP3A USIC LOCATING SERVICE-ROCKFOR Information not provided



October 12,2010

RECEIVED

OCT 21 2010

Wills Burke Kelsey Associates

Mr. Kevin Anderson, P.E.,PTOE  
Wills Burke Kelsey Associates  
116 West Main Street, Suite 201  
St. Charles,Illinois 60174

RE: Walker Road over Burlington Creek/French Rd over Burlington Creek  
Kane County Dept. of Transportation  
Sec. 08-00386-00-BR  
Job No.: P91-270-09  
Project No.: BROS-0089(147/155)

Dear Mr. Anderson:

Please find enclosed, a redline copy of your plans showing the location of AT&T facilities along Walker Rd. There is one buried cable along the west side of the road approx. 3' from the RW and crossing to the east and then south at sta 14+00. I have also attached a copy of a pedestal location on the SE corner of Allen & Walker due to the small size of the provided plans. I have also provided a sketch of AT&T facilities locations on French Rd. with approx. distances from the edge of pavement.

If I can be of any further assistance, please contact me at 815-394-7297.

Sincerely yours,

David Saint Germain  
Legal Mandate Engineer  
815-394-7297

ComEd  
Rockford Business Office  
123 Energy Avenue  
Rockford, IL 61109-1099

www.exeloncorp.com

September 10, 2010

Kevin Anderson, P.E.  
Wills Burke Kelsey Associates  
116 West Main St., Suite 201  
St. Charles, IL 60174

Re: H11046DKB - Walker Rd over Burlington Creek - Kane County

Dear Kevin,

This letter is to notify you that we have received your request, dated September 7, 2010, for ComEd to review the subject area and provide a response. ComEd will review the project area and research existing facilities and easements to determine potential conflict and associated costs to the requestor, if applicable.

In an effort to support your project successfully, the following will be required prior to beginning Engineering Design:

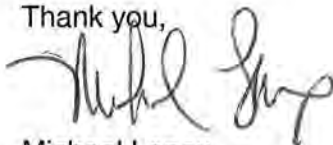
1. Letter from requesting agency stating expected relocation completion date and your direction for ComEd to proceed with engineering design.
2. Stamped Pre-final or Final Plans submitted with your letter of direction.
3. Agency's anticipated construction start and finish date.

Upon receiving the above-mentioned letter and plans, the following timelines are required to relocate our facilities:

1. 4 to 10 weeks for engineering design, then
2. 10 to 12 weeks prior to ComEd construction start for scheduling, ordering materials, securing resources, coordinating customer outages and attaining permits.
3. Construction duration depends on amount of relocation required. Typically 3 to 4 weeks for 7 poles or less. However, overall timeline may vary based upon scope and complexity of work required.

Upon review of your submitted pre-final or final plans and your letter of direction to have ComEd proceed with relocation, I will work with you to schedule and coordinate our activities. If I can answer any questions please do not hesitate to contact me.

Thank you,



Michael Lenox  
Public Relocation Department  
(815)490-2869  
michael.lenox@comed.com

Cc: Mike Zakosek - Kane County DOT





### Project Review Response

Subject: <b>Walker Rd over Burlington Creek</b>	Project #: <b>H11045DKB</b>
Section: <b>08-00133-01-BR</b>	P.L. #: <b>420-20N</b>
Contract:	Town: <b>Hampshire Twp</b>
	County: <b>Kane</b>

September 10, 2010

To: Kevin Anderson, P.E.  
Wills Burke Kelsey Associates

From: Mike Lenox  
Public Relocation Group  
123 Energy Ave.  
Rockford, IL 61109

This is in response to your letter dated September 7, 2010.

Distribution Engineering has reviewed plans for the above mentioned project. This project conflicts with our distribution facilities as indicated below:

- Existing \_\_\_\_ kV distribution facilities have been marked in red on the enclosed plans.
- There are no existing overhead facilities in the subject area.
- There are existing **12** kV overhead facilities and **unknown** poles in conflict with the subject improvement. The contractors should use caution in operating cranes or other equipment near our overhead facilities.
- There are no existing underground facilities in the subject area.
- There are existing \_\_\_\_ kV underground/conduit facilities in the subject area. We request that J.U.L.I.E. be contacted 72 hours prior to the start of construction.
- The proposed work should not conflict with our distribution facilities.
- Construction will be scheduled to begin approximately 20 weeks from receipt of detailed plans and letter of direction from Kane County DOT.
- It will take approximately \_\_\_\_ construction days to clear the conflicts.

If additional information is required, please contact Mike Lenox at (815) 490-2869 or michael.lenox@comed.com.

Remarks:

ComEd has an overhead line along the east side of Walker Rd through the project limits.



Nicor Gas  
1844 Ferry Road  
Naperville, IL 60563-9600

Mailing Address:  
P.O. Box 190  
Aurora, IL 60507-0190

Phone 630 983-8676  
Internet www.nicor.com

September 14, 2010

Mr. Kevin Anderson, P.E., PTOE  
Wills Burke Kelsey Associates  
116 West Main Street, Ste 201  
St. Charles, Illinois 60174

Re: **09-0056 – Walker Road over Burlington Creek**  
**Section No. 08-00133-01-BR**  
**JULIE Dig # A1801496**

Dear Mr. Anderson

Thank you for your fax/and or letter and plans received on September 13, 2010. Our atlas pages indicate that there are no Nicor Gas mains in the area of your proposed construction.

Please phone JULIE, 1.800.892.0123, 48 hours prior to construction for location of our facilities within your proposed improvement.

Your project has been assigned Engineering #**N7717**. Please refer to this number in all future correspondence to assist with expediting any future inquiry.

Thank you for your cooperation in this matter.

Sincerely,

A handwritten signature in cursive script that reads "Constance Lane/ka".

Constance Lane  
Engineering Administrator  
Phone: (630) 388-3830  
Fax: 630.983.0639

**N7717**

**APPENDIX G**  
**PUBLIC INVOLVEMENT**

WILLS BURKE KELSEY ASSOCIATES, LTD.  
Public Hearing Offer

ADORDERNUMBER: 0000499898-01  
PO NUMBER: Public Hearing Offer  
AMOUNT: \$134.64  
NO OF AFFIDAVITS: 1

# Sun Times Media Elgin Courier-News Certificate of Publication

State of Illinois - County of DeKalb, Kane

Elgin Courier-News, does hereby certify it has published the attached advertisements in the following secular newspapers. All newspapers meet Illinois Compiled Statute requirements for publication of Notices per Chapter 715 ILCS 5/0.01 et seq. R.S. 1874, P728 Sec 1, EFF. July 1, 1874. Amended by Laws 1959, P1494, EFF. July 17, 1959. Formerly Ill. Rev. Stat. 1991, CH100, PI.

Note: Notice appeared in the following checked positions.

PUBLICATION DATE(S): 02/08/2013, 02/15/2013

Elgin Courier News

### Public Hearing Offer

The Kane County Division of Transportation is proposing to improve the Walker Road Bridge over Burlington Creek located in unincorporated Kane County between Allen Road and Illinois Route 72. The project will consist of the removal and replacement of the existing bridge. The approach roadway will be widened to accommodate two 12-foot lanes with 4-foot shoulders.

During construction, the County proposes to close Walker Road at Burlington Creek. A detour route will be marked on Allen Road, Illinois Route 72, and State Street in the Village of Hampshire.

Any interested person who believes a public hearing should be held for this project may request that a hearing be held by contacting Mike Zakosek, Senior Project Manager, Kane County Division of Transportation, 41W011 Burlington Road, St. Charles, IL 60174, telephone 630-406-7346. To be considered, requests must be received by March 1, 2013. A hearing will be held if public response demonstrates sufficient interest.

An exhibit depicting the project area is available for viewing by the general public at the office at the address listed above.  
499898 Pub: 2/8 & 2/15/2013

WILLS BURKE KELSEY ASSOCIATES  
RECEIVED  
FEB 22 2013

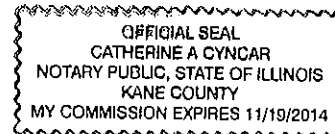
IN WITNESS WHEREOF, the undersigned, being duly authorized, has caused this Certificate to be signed and notarized

By   
David Fontechia  
Account Manager - Public Legal Notices

Subscribed and sworn to before me this 15th Day of February 2013 A.D.

  
Catherine A. Cyncar  
Notary Public

WILLS BURKE KELSEY ASSOCIATES, LTD.  
116 WEST MAIN STREET, SUITE 201  
SAINT CHARLES, IL 60174



# Order Confirmation

Ad Order Number  
0000499898

Customer  
WILLS BURKE KELSEY ASSOCIATES, LTD., WILLS BURKE KELSEY A/

Payor Customer  
WILLS BURKE KELSEY A/

PO Number  
Public Hearing Offer

Sales Rep.  
kfranzen

Customer Account  
100247255

Payor Account  
100247255

Ordered By  
Andy Underwager

Order Taker  
kfranzen

Customer Address  
116 WEST MAIN STREET, SUITE 201  
SAINT CHARLES IL 60174 USA

Payor Address  
116 WEST MAIN STREET, SUITE 201  
SAINT CHARLES IL 60174 USA

Customer Fax  
630-443-0533

Order Source  
06 Email

Customer Phone  
630-443-7755

Payor Phone  
630-443-7755

Customer EMail

Special Pricing  
None

<u>Tear Sheets</u>	<u>Proofs</u>	<u>Affidavits</u>	<u>Blind Box</u>	<u>Promo Type</u>	<u>Materials</u>
0	0	1			

<u>Invoice Text</u>	<u>Ad Order Notes</u>	<u>Net Amount</u>	<u>Tax Amount</u>	<u>Total Amount</u>	<u>Payment Method</u>	<u>Payment Amount</u>	<u>Amount Due</u>
Public Hearing Offer		\$134.64	\$0.00	\$134.64	Credit Card - Visa:9421	\$134.64	\$0.00

<u>Ad Number</u>	<u>Ad Type</u>	<u>Ad Size</u>	<u>Color</u>	<u>Production Method</u>	<u>Production Notes</u>
0000499898-0	IL Legal Liner	1.0 X 44 LI	<NONE>	AdBooker	

<u>External Ad Number</u>	<u>Ad Attributes</u>	<u>Ad Released</u>	<u>Pick Up</u>
		No	

<u>Product Information</u>	<u>Placement/Classification</u>	<u>Run Dates</u>	<u># Inserts</u>	<u>Cost</u>
<u>Run Schedule Invoice Text</u>	<u>Sort Text</u>			
LegalWeb::Full Run	Legals	2/8/2013, 2/15/2013	2	\$0.00
Public Hearing Offer	PUBLIC HEARING OFFER	2/8/2013, 2/15/2013	2	\$134.64
Elgin Courier News::Full Run	Legals			
Public Hearing Offer	PUBLIC HEARING OFFER			

# KANE COUNTY

## DIVISION of TRANSPORTATION

Carl Schoedel, P.E.  
Director of Transportation  
County Engineer



41W011 Burlington Road  
St. Charles, IL 60175  
Phone: (630) 584-1170  
Fax: (630) 584-5265

March 23, 2013

Mr. William L. Dumoulin  
16N393 Walker Road  
Hampshire, IL 60140-8260

Re: Walker Road over Burlington Creek – Bridge Replacement

Dear Mr. Dumoulin:

The Kane County Division of Transportation is engaged in the preliminary engineering and environmental studies phase for the improvement of the Walker Road bridge over Burlington Creek in unincorporated Kane County. This improvement is included in the Transportation Improvement Program and is scheduled for construction in 2014, subject to funding availability and project readiness. The proposed improvement consists of removal and replacement of the existing bridge and approximately 500 feet of pavement reconstruction on each side of the proposed bridge. This work will include new shoulder and guardrail.

It is the policy of the Kane County Division of Transportation and the Illinois Department of Transportation to give all interested persons an opportunity to become acquainted with highway proposals of concern to them. Those interested and/or concerned persons are encouraged to express their views at those stages of a proposed project when the flexibility to respond still exists. The current practice is to contact each affected property owner directly by mail.

Based upon a review of the year 2012 tax records of Kane County, you are indicated to be the owner of 01-19-200-003. This property is situated along the westerly limits of the proposed bridge improvement as shown on the attached plan sheet.

The proposed improvement will require a small amount of right-of-way (ROW) and Temporary Easement (TE) from your property. The proposed right of way acquisition of 0.464 acres to be acquired from your property is needed for access and grading and is indicated in orange on the enclosed plan sheet. The additional 0.081 acres, denoted in blue, is the proposed temporary easement needed for grading the ditches and shoulders.

In order to further assist us in our analysis of project impacts, please indicate on the attached comment form the locations of any wells, septic systems, sprinkler systems, or other improvements on your property that may be affected by our project. The form at

the end of this letter can also be used to provide any other comments you may have, or to request further discussions with us, either via telephone or in a personal meeting. Please indicate the response most convenient for you. A second copy of this comment page is attached for your records. Please return a signed copy of the comment page to us in the enclosed, self-addressed, stamped envelope. Even if you have no comments regarding the project at this time, please return this form indicating if there are potentially impacted improvements on your property.

Upon completion of our study, a project report will be prepared describing the proposed work and submitted to the Illinois Department of Transportation for approval. After approval is received we will proceed into the plan preparation and land acquisition phase. In that phase, a representative of the Kane County Division of Highways will contact you regarding any necessary acquisition. Please note that your response, or lack thereof, will in no way influence the amount of compensation you may receive for your property if acquired as part of the project.

Upon receipt of this letter, we request your response within 10 days, so that we can effectively maintain our project schedule. If, after reviewing this letter, you have any questions or wish to arrange a meeting to discuss this improvement in more detail, please free to contact me.

Respectfully,



Michael Zakosek, P.E.  
Senior Project Manager

Enclosures

cc: Andy Underwager – Wills Burke Kelsey Associates

Walker Road over Burlington Creek  
Section 08-00133-01-BR  
Kane County

RE: Property Tax Number 01-19-200-003

Please check the desired response.

\_\_\_\_\_ I have no comments at this time.

\_\_\_\_\_ I have noted my comments on the back of this page.

\_\_\_\_\_ I will call the project manager/engineer at (630) 406-7346 to either discuss this further in a telephone conversation or to arrange a personal meeting to discuss this project.

\_\_\_\_\_ Please call me at \_\_\_\_\_ to discuss this further in a phone conversation (your telephone number). The best time to reach me is at \_\_\_\_\_ . (Preferred time and date)

\_\_\_\_\_ There are improvements on my property that may be impacted by the proposed improvement. The improvements include (also specify approximate location):

- Well
- Septic System
- Fence
- Advertising Sign
- Other \_\_\_\_\_

\_\_\_\_\_  
Signature of owner

\_\_\_\_\_  
Date

2013



**KANE COUNTY**  
DIVISION of TRANSPORTATION

Carl Schoedel, P.E.  
Director of Transportation  
County Engineer



41W011 Burlington Road  
St. Charles, IL 60175  
Phone: (630) 584-1170  
Fax: (630) 584-5265

March 23, 2013

Ms. Melanie J. Howe  
16N731 Walker Road  
Hampshire, IL 60140-8228

Re: Walker Road over Burlington Creek – Bridge Replacement

Dear Ms. Howe:

The Kane County Division of Transportation is engaged in the preliminary engineering and environmental studies phase for the improvement of the Walker Road bridge over Burlington Creek in unincorporated Kane County. This improvement is included in the Transportation Improvement Program and is scheduled for construction in 2014, subject to funding availability and project readiness. The proposed improvement consists of removal and replacement of the existing bridge and approximately 500 feet of pavement reconstruction on each side of the proposed bridge. This work will include new shoulder and guardrail.

It is the policy of the Kane County Division of Transportation and the Illinois Department of Transportation to give all interested persons an opportunity to become acquainted with highway proposals of concern to them. Those interested and/or concerned persons are encouraged to express their views at those stages of a proposed project when the flexibility to respond still exists. The current practice is to contact each affected property owner directly by mail.

Based upon a review of the year 2012 tax records of Kane County, you are indicated to be the owner of 01-20-100-001. This property is situated along the easterly limits of the proposed bridge improvement at 16N731 Walker Road, as shown on the attached plan sheet.

The proposed improvement will require a small amount of right-of-way (ROW) and Temporary Easement (TE) from your property. The proposed right of way acquisition of 0.452 acres to be acquired from your property is needed for access and grading and is indicated in orange on the enclosed plan sheet. The additional 0.097 acres, denoted in blue, is the proposed temporary easement needed for grading the ditches and shoulders.

In order to further assist us in our analysis of project impacts, please indicate on the attached comment form the locations of any wells, septic systems, sprinkler systems, or

other improvements on your property that may be affected by our project. The form at the end of this letter can also be used to provide any other comments you may have, or to request further discussions with us, either via telephone or in a personal meeting. Please indicate the response most convenient for you. A second copy of this comment page is attached for your records. Please return a signed copy of the comment page to us in the enclosed, self-addressed, stamped envelope. Even if you have no comments regarding the project at this time, please return this form indicating if there are potentially impacted improvements on your property.

Upon completion of our study, a project report will be prepared describing the proposed work and submitted to the Illinois Department of Transportation for approval. After approval is received we will proceed into the plan preparation and land acquisition phase. In that phase, a representative of the Kane County Division of Highways will contact you regarding any necessary acquisition. Please note that your response, or lack thereof, will in no way influence the amount of compensation you may receive for your property if acquired as part of the project.

Upon receipt of this letter, we request your response within 10 days, so that we can effectively maintain our project schedule. If, after reviewing this letter, you have any questions or wish to arrange a meeting to discuss this improvement in more detail, please free to contact me.

Respectfully,



Michael Zakosek, P.E.  
Senior Project Manager

Enclosures

cc: Andy Underwager – Wills Burke Kelsey Associates

Walker Road over Burlington Creek  
Section 08-00133-01-BR  
Kane County

RE: Property Tax Number 01-20-100-001

Please check the desired response.

\_\_\_\_\_ I have no comments at this time.

\_\_\_\_\_ I have noted my comments on the back of this page.

\_\_\_\_\_ I will call the project manager/engineer at (630) 406-7346 to either discuss this further in a telephone conversation or to arrange a personal meeting to discuss this project.

\_\_\_\_\_ Please call me at \_\_\_\_\_ to discuss this further in a phone conversation (your telephone number). The best time to reach me is at \_\_\_\_\_ . (Preferred time and date)

\_\_\_\_\_ There are improvements on my property that may be impacted by the proposed improvement. The improvements include (also specify approximate location):

- Well
- Septic System
- Fence
- Advertising Sign
- Other \_\_\_\_\_

\_\_\_\_\_  
Signature of owner

\_\_\_\_\_  
Date

2013

Walker Road over Burlington Creek  
Section 08-00133-01-BR  
Kane County

RE: Property Tax Number 01-19-200-003

Please check the desired response.

       I have no comments at this time.

  X   I have noted my comments on the back of this page.

  X   I will call the project manager/engineer at (630) 406-7346 to either discuss this further in a telephone conversation or to arrange a personal meeting to discuss this project.

  X   Please call me at 847-815-7919 to discuss this further in a phone conversation (your telephone number). The best time to reach me is at ANYTIME. (Preferred time and date)  
I CARRY MY PHONE W/ME MUCH OF THE TIME - THANKS -

       There are improvements on my property that may be impacted by the proposed improvement. The improvements include (also specify approximate location):

- Well
- Septic System
- Fence
- Advertising Sign
- Other \_\_\_\_\_



Signature of owner

3-27-2013  
Date

2013

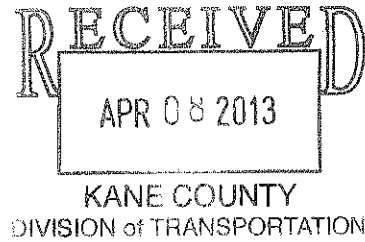
*Called Mike ZAKOBEK  
2:30 PM  
3-27 - LEFT MESSAGE*

*TAKE TO MARY  
C 2:45 PM  
3-27  
& SHE WILL MAKE SURE  
MIKE GETS THIS PAPER*

THE ~~THE~~ "NEW" GUARD RAILS THAT WERE INSTALLED  
ARE A MAJOR PROBLEM BECAUSE OF FARM  
EQUIPMENT TODAY. WHEN I'M AM STOPPED WAITING  
FOR TRAFFIC IT IS VERY DIFFICULT TO GET STARTED  
AND THROUGH ALL THE GUARD RAILS BEFORE  
THE NEXT CARS COME THROUGH, THE CARS  
HAVE TO BE AT LEAST AT THE TRACKS  
BEFORE YOU CAN START BACK UP AGAIN TO  
ALLOW FOR A "SAFE" DISTANCE & THAT ALSO  
SAVES THAT NO OTHER CARS TRY TO PASS  
YOU BEFORE I PULL OUT, THEN WE HAVE  
ALSO LOST ACCESS TO OUR FIELD. PLEASE  
CONTACT <sup>US</sup> SO THAT WE CAN VISIT & EXPRESS  
OUR CONCERNS TO HELP DETER ANY  
ACCIDENTS WHICH MIGHT INVOLVE  
FARM EQUIPMENT. THANKS FOR YOUR TIME

DUMOULIN FARMS  
MIKE 847-815-7919  
PAT 847-815-7921

Walker Road over Burlington Creek  
Section 08-00133-01-BR  
Kane County



RE: Property Tax Number 01-20-100-001

Please check the desired response.

\_\_\_ I have no comments at this time.

\_\_\_ I have noted my comments on the back of this page.

X I will call the project manager/engineer at (630) 406-7346 to either discuss this further in a telephone conversation or to arrange a personal meeting to discuss this project.

\_\_\_ Please call me at \_\_\_\_\_ to discuss this further in a phone conversation (your telephone number). The best time to reach me is at \_\_\_\_\_. (Preferred time and date)

\_\_\_ There are improvements on my property that may be impacted by the proposed improvement. The improvements include (also specify approximate location):

Well

Septic System

Fence

Advertising Sign

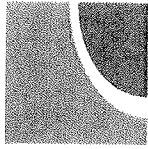
Other Field Entrances + if Timing of project will coincide with either planting or Harvesting

Melanie J. Howe  
Signature of owner

4/5/2013  
Date

2013

**APPENDIX H**  
**OTHER AGENCY COORDINATION**



Chicago Metropolitan  
Agency for Planning

233 South Wacker Drive  
Suite 800  
Chicago, Illinois 60606  
  
312 454 0400  
www.cmap.illinois.gov

September 16, 2011

Mr. Carl Schoedel, P.E.  
County Engineer  
Kane County Division of Transportation  
41W011 Burlington Road  
St. Charles, IL 60504

**Subject: Bridge Replacements - Bliss Road, Walker Road and French Road**  
Kane County DOT

Dear Mr. Schoedel:

In response to a request made on your behalf and dated September 14, 2011, we have developed year 2040 average daily traffic (ADT) projections for the subject locations.

ROAD SEGMENT	2040 ADT
Bliss Rd over Blackberry Creek	12,000
Walker Rd over Burlington Creek	6,000
French Rd over Burlington Creek	7,000

Traffic projections are developed using existing ADT data provided in the request letter and the results from the April 2011 CMAP RTP/TIP Travel Demand Analysis. The regional travel model uses CMAP 2040 socioeconomic projections and assumes the implementation of the 2040 Regional Transportation Plan for the Northeastern Illinois area.

If you have any questions, please call Jose Rodriguez at (312) 386-8806.

Sincerely,

Donald P. Kopec  
Deputy Director for Planning and Programming

cc: Anderson (Wills Burke Kelsey)  
M:\proj\ncb\forecasts\2011 Response\ka-11-11.docx

RECEIVED  
SEP 20 2011

Wills Burke Kelsey Associates





**Record of Conversation**

<b>Person Contacted</b>	John Hortness USGS, Ill. Water Science Center, DeKalb	<b>Date</b>	9/14/10
<b>Phone No.</b>	815-756-8207	<b>Time</b>	9:30 AM
<b>By</b>	Kevin Anderson, WBKA		
<b>Project</b>	Walker Road		
<b>Subject</b>	USGS Crest Stage Monitoring Station		

---

The USGS turned that monitoring station over to Kane County about a year ago. The contact person is:

Karen Kosky  
630-208-8665  
Division of Environmental and Building Management

## Kevin Anderson

---

**From:** Harbaugh, Tim [harbaughtim@co.kane.il.us]  
**Sent:** Tuesday, September 14, 2010 3:50 PM  
**To:** Kosky, Karen; Kevin Anderson  
**Cc:** Harbaugh, Tim  
**Subject:** RE: USGS Crest Stage Monitoring Station- Walker Road Bridge at Burlington Creek

No need that I am aware of to maintain this gage.

Tim Harbaugh, P.E., DEE  
Director of Facilities, Subdivision, and Environmental Resources  
719 Batavia Ave.  
Geneva, Il., 60134  
(630)208-5173  
(630)208-5137 fax  
email: [harbaughtim@co.kane.il.us](mailto:harbaughtim@co.kane.il.us)  
website: [www.co.kane.il.us](http://www.co.kane.il.us)

**IMPORTANT NOTICE:** This e-mail (including any attachments) is covered by the Electronic Communications Privacy Act, 18 U.S.C. Sections 2510-2521, is confidential, and may contain information or material that is confidential, privileged and protected from disclosure under applicable laws. This e-mail is intended only for those individuals and entities to which it is addressed. If you are not the intended recipient, you are hereby notified that any retention, dissemination, distribution or copying of this communication (including any attachments) is strictly prohibited. If you are not the intended recipient, please reply to the sender that you have received the message in error, then delete it and any and all copies of it immediately. Thank you.

---

**From:** Kosky, Karen  
**Sent:** Tuesday, September 14, 2010 2:20 PM  
**To:** Kevin Anderson  
**Cc:** harbaughtim@co.kane.il.us  
**Subject:** RE: USGS Crest Stage Monitoring Station- Walker Road Bridge at Burlington Creek

Kevin:

Kane County has not continued monitoring of those crest-stage gages during the last year. So we will not require any sort of coordination during construction & removal of this gage.

Tim, please advise whether you want KDOT to have this crest-stage gage replaced upon bridge completion, or if we should just have it permanently removed.

Thanks,  
Karen Kosky

---

**From:** Kevin Anderson [mailto:kanderson@wbkengineering.com]  
**Sent:** Tuesday, September 14, 2010 1:58 PM  
**To:** Kosky, Karen  
**Subject:** USGS Crest Stage Monitoring Station- Walker Road Bridge at Burlington Creek

Karen,

I am following up my voicemail with additional information. WBK is assisting KDOT with the preparation of Phase I Engineering plans for the Walker Road over Burlington Creek bridge replacement project (anticipated construction: 2013). There is a USGS Crest Stage Monitoring Station at the bridge. John Hortness of the USGS told me that the monitoring station was transferred to Kane County about a year ago and he gave me your name.

We are in the stage of the design process where we need to identify items that require coordination with other agencies. The existing monitoring station will require removal during construction. We need to know what sort of coordination your agency will require (such as: advance notice required before removal, whether the contractor or County forces should remove it, whether we need to designate a location for a replacement, and who will be responsible for installing the replacement). For the Phase I design, we need to document what process should be followed regarding the monitoring station in advance of, during, and after construction.

Please let me know what actions need to be taken regarding this monitoring station. The information you give me will be documented in the Phase I Engineering Design Report.

Thank you,

Kevin Anderson, P.E., P.T.O.E.

**Wills Burke Kelsey Associates**

116 West Main Street, Suite 201

St. Charles, IL 60174-1854

Ph. 630-443-7755

[KAnderson@wbkengineering.com](mailto:KAnderson@wbkengineering.com)